

April 2016

Project Development



Bicycle and Pedestrian Bridge over the North
Channel of the Boise River

Eagle Road/Highway 55

FINAL

Prepared for:



COMPASS
COMMUNITY PLANNING ASSOCIATION
of Southwest Idaho

Project Location:

Eagle, Idaho

Prepared by:

J-U-B ENGINEERS, Inc.

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Executive Summary

This project was prompted by the City of Eagle and private citizens to develop a concept that would provide north-south connectivity along Eagle Road/Highway 55 to existing and future greenbelt pathways and neighborhoods throughout a highly-traveled area. Prior to developing this document, a feasibility study that explored multiple crossing options was completed in October 2015. The Bridge Department of the Idaho Transportation Department (ITD) reviewed the feasibility study and concurred with its findings and recommendations.

This document includes conceptual project information for a proposed bike/pedestrian bridge attached to the existing Eagle Rd/SH-55 Bridge crossing the north channel of the Boise River in Eagle, Idaho. J-U-B ENGINEERS, Inc. (J-U-B) was hired by the Community Planning Association of Southwest Idaho (COMPASS) to compile this information. The purpose of this document is to transform COMPASS member agency needs into well-defined projects with cost estimates, purpose and need statements, environmental scans, and public involvement plans to ensure readiness for funding applications. Well-defined and scoped projects strengthen grant applications and, once funded, increase the probability of projects to be delivered on time and on budget.

The estimated project cost is approximately \$1,299,000, and the primary funding source would likely be the Transportation Alternatives Program (TAP), also known as state Surface Transportation Program Set-Aside (STP Set-Aside). Additional funding options and match scenarios are identified in the Funding section of this report. Due to the cost of the project, it will take multiple funding sources, including local matching dollars (can be public and private) to implement this project. Although it will take some time and work to get through the process and secure funding, strong public support and ongoing collaboration efforts will play a key role in turning this vision into reality.

As indicated in this Report, the City of Eagle will be responsible for securing funding, coordinating with stakeholders and the bridge committee (in the process of being formed), obtaining approval from ITD for the use of existing right-of-way (ROW), and ultimately owning and maintaining the proposed bike/pedestrian bridge. It is recommended that coordination efforts continue between the City of Eagle and COMPASS as funding is secured and when any additional project details and/or technical questions arise.

Introduction

Purpose of This Report

The purpose of this document is to provide key pieces of information to the City of Eagle to apply for grants in the near future. It also provides guidance for ongoing stakeholder collaboration to keep the project moving forward.

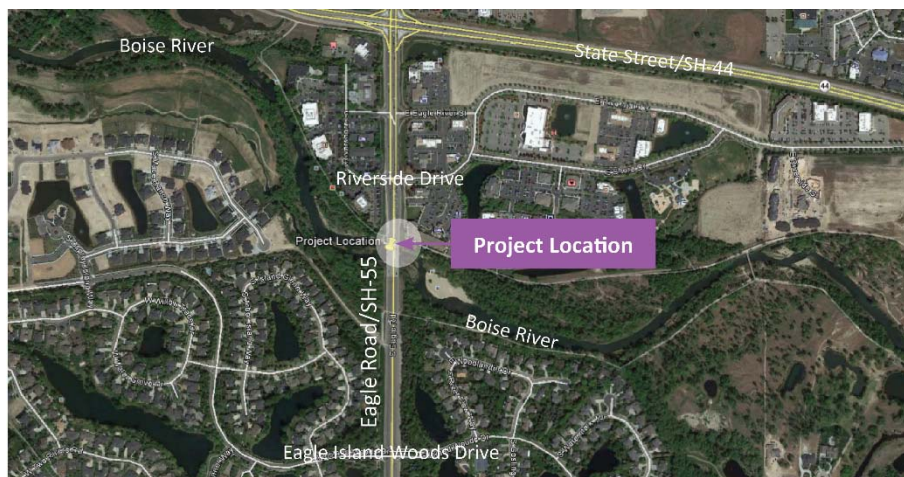
This project was prompted by the City of Eagle and private citizens to develop a concept that would provide north-south connectivity along Eagle Road/Highway 55 to existing and future greenbelt pathways and neighborhoods throughout a highly-traveled area. Prior to developing this document, a feasibility study that explored multiple crossing options was completed in October 2015. The Bridge Department of ITD reviewed the feasibility study and concurred with its findings and recommendations.

This effort is the result of an approved request made by the City of Eagle through COMPASS' Project Development program which is funded with federal planning funds provided by the Federal Highway Administration (FHWA).

Project Description

Background

The City of Eagle (City); Walk and Ride Eagle, LLC; Two Rivers Community; and ITD have collaborated for a proposed bicycle pedestrian bridge adjacent to the west side of the existing Eagle Road/State Highway 55 (SH-55) Bridge (the bridge) over the north channel of the Boise River (the river). This project will provide north/south connectivity of existing bike/pedestrian paths in the surrounding communities as well as overall connectivity with the local neighborhoods, businesses, public recreation facilities and with downtown City of Eagle. This new bike/pedestrian bridge will provide critical linkages between the Boise River Greenbelt, planned pathways, and a rapidly growing residential and commercial core area.



*Figure 1:
Project
Location
Map*

Currently, there is no safe or practical designated north-south bike/pedestrian river crossing on the existing vehicular bridge over the river. Bike/pedestrian use of the existing bridge shoulders on SH-55 has increased and is expected to continue to increase, given the rapid growth and development in neighborhoods and businesses in this area.

The existing bridge consists of five lanes (four thru-lanes and one center lane) and shoulders on both sides. The function of the center lane is to provide adequate room for vehicle stacking at the intersections of Riverside Drive to the north and Island Woods Drive to the south.



Figure 2: Citizens walking along the shoulder of the SH-55 bridge (June 2015)

The existing bridge consists of three spans at 75 ft. – 90 ft. - 75 ft. (for a total length of 240 ft.) of precast concrete beams with a cast in place concrete deck. Each span is supported by concrete abutments and piers supported on driven steel piles. The existing bridge was constructed in two phases. The east half of the bridge was constructed in 1974 and the west half of the bridge was constructed in 1996. In general, the bridge is in good condition. However, the typical design life for a bridge such as this is approximately 75 years, which gives a remaining theoretical service life of 33 years based on the original construction year. The results of the October 2015

feasibility study took into consideration the remaining service life of the existing bridge when evaluating options that would involve attaching to the existing bridge. During the feasibility study process, ITD indicated that the proposed bike/pedestrian bridge would be included in any future ITD project requiring the replacement of the existing highway bridge.

Several key factors were investigated to determine the recommended bike/pedestrian bridge location, size and type. The location and length of the proposed bike/pedestrian bridge and location of the proposed bike/pedestrian bridge piers were selected to eliminate and/or minimize hydraulic and environmental impacts. The location of the bridge also required careful consideration, including ways to minimize impacts to existing facilities and utilities adjacent to the bridge, and to avoid conflict with existing bridge maintenance activities performed by ITD. The width of the proposed bike/pedestrian bridge was determined to accommodate two-way bike/pedestrian traffic and meet ADA requirements.

Location

The northeast corner of the bridge is adjacent to a hotel and utilities such as overhead power (**Figure 3**), all of which are closely located to the east side of the bridge. The east side of the bridge is also on the upstream side of the river, where debris tends to collect and lodges against the in-stream piers. The east side is typically where the majority of bridge maintenance activity occurs.

The proposed bike/pedestrian bridge will be located on the west side of the bridge to avoid conflicts and impacts during construction of the bike/pedestrian bridge, and to avoid the impacts while the completed bike/pedestrian bridge is in service as mentioned in the previous paragraph.



Figure 3: East side of SH-55 bridge, utilities, hotel, pathway connection (June 2015) Looking North at "Point C" on Figure 4, "Bridge Plan View"

Locating the new bridge on the west side (**Figure 4**, Point A to Point B) allows for a direct north-south crossing of the River on the west side. There are two existing bike/pedestrian underpasses that bicyclists and pedestrians can utilize to cross Eagle Road from the east side to access the proposed north-south crossing located on the northwest and southwest sides of the Boise River. **Figure 4**, Point C to Point B shows the North Underpass and Point D to Point E South Underpass. These existing bike/pedestrian underpasses, coupled with the existing greenbelt and the new bridge, will provide the community with safe and convenient access to and through Eagle's core. Signing and traffic calming measure will be used at the west side of the existing South Underpass and at the "tie-in" point at the northwest corner of the proposed bridge.

The northwest and southwest corners of the existing bridge are well suited to accommodate the abutments for the bike/pedestrian bridge to match the existing pathway grades while also meeting ADA requirements. A future parking area will be constructed about 200 feet south of the South Underpass, on the west side of Eagle Road (**Figure 4**, Point F).

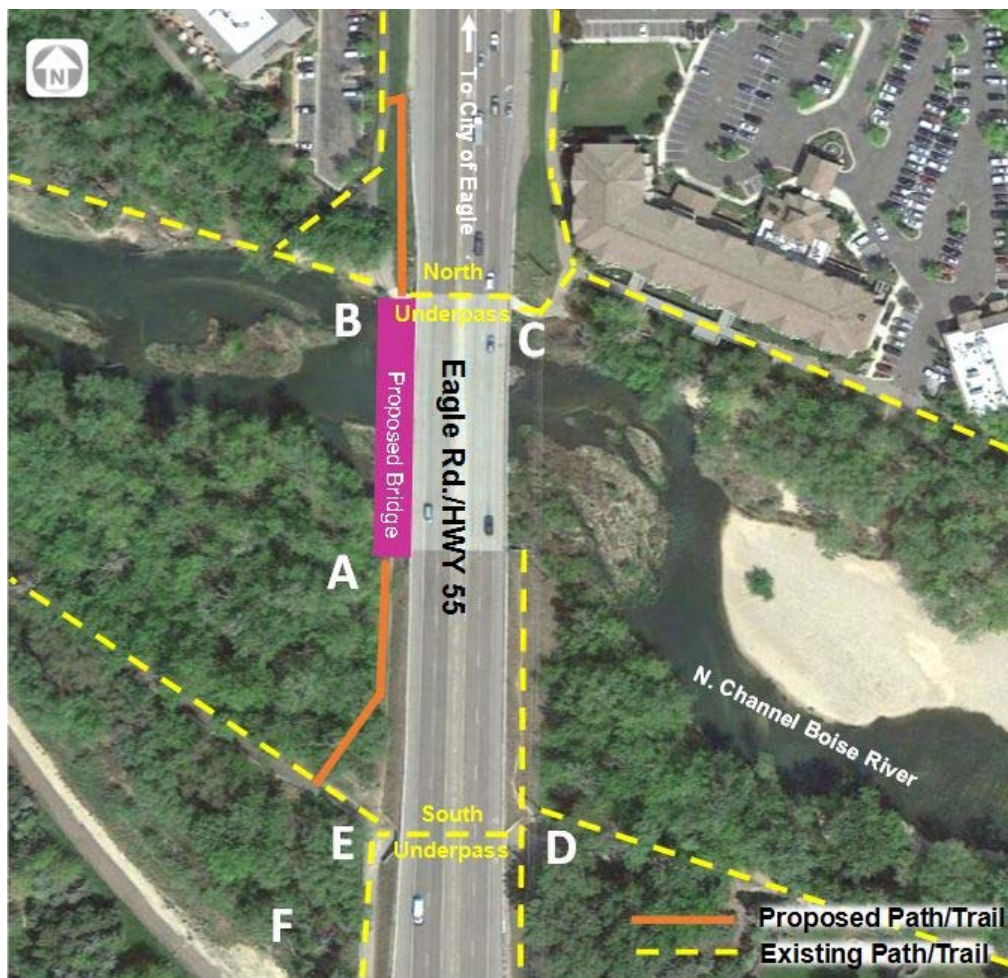


Figure 4: Bridge Plan View (existing path with reference Points A, B, C, D, E and F)

Figure 5 shows the southwest corner of the existing abutments in the background of the instream piers. **Figure 6** shows the existing bike/pedestrian path as it crosses under the northwest corner of the existing bridge. As shown in both of these photos, there are no significant conflicts with any existing facilities or utilities on the west side of the bridge.



Figure 5: West side of SH-55 bridge, (June 2015) view looking south standing at "Point B" shown on Figure 4, "Bridge Plan View"



Figure 6: West side of SH-55 bridge, (June 2015) view looking at the NW corner "Point B" shown on Figure 4, "Bridge Plan View"

Figure 7 shows the northwest corner of the existing bridge, where the proposed bike/pedestrian bridge abutment would be located. Note that the individual in **Figure 7** is standing on the existing east-west path (Point B on Figure 4), and is pointing northward at the approximate location where the path on the proposed bridge would tie in with the existing top of embankment, the existing sidewalk at the top of the embankment, and the existing east-west bike/pedestrian path.



Figure 7: West side of SH-55 bridge, north abutment (June 2015) at northwest corner, "Point B" on Figure 4 - "Bridge Plan View", looking north

Size and Type of Proposed Bike/Pedestrian Bridge

Multiple bridge types and sizes were investigated during the Feasibility Study process. A lightweight aluminum bridge attached and supported by the existing bridge was studied to determine if the need for additional foundation piles and concrete piers could be avoided to minimize the cost of the bridge. It was found that a lightweight aluminum structure would not be light enough to be supported by the existing bridge piles and would require additional piles and concrete piers. Therefore, the added cost of using a lightweight aluminum structure was of no advantage compared to less costly bridge types such as concrete beams or steel beams. A single span steel truss bridge was also investigated. This option was determined to be cost prohibitive and was also determined unfeasible.

Options to use a three span truss, a three span steel beam and a three span concrete pedestrian bridge were analyzed and compared. It was found that the three span concrete bridge would be the most cost effective option and was selected as the preferred option.

The three span concrete option seems to harmonize with the existing bridge and surroundings because it is similar to the existing concrete bridge beams and has the same low profile. **Figure 8** and **Figure 9** gives a “before and after” view at the northwest corner of the bridge. **Figure 9** is a rendering of the proposed bike/pedestrian bridge with precast beams supported on new piers constructed up against the existing piers. A proposed reinforced soil wall is shown as a rendering at the northwest abutment for the proposed bridge.



Figure 8: 'Before' photo

Figure 9: 'After' rendering – concrete beams supported on new piers, adjacent to existing bridge, looking southeast



The length of the bike/pedestrian bridge will be the same as the existing 240 ft. long bridge to maintain the existing channel opening and to accommodate the existing east-west path at the north side of the river. The bike/pedestrian bridge will also have the same span lengths as the existing bridge in order to maintain the existing stream hydraulic behavior, maintain a “no-rise” impact on the river for the additional in-stream bike/pedestrian piers, and to minimize any changes that would increase the potential for scouring at the existing abutments and piers. The abutments and piers for the proposed bike/pedestrian bridge would be constructed against the existing bridge abutments and piers while maintaining structural independence from each other with small joints between the existing and proposed structures. **Figure 10** and **Figure 11** are “before and after” views of the in-stream pier being viewed from the NW corner of the bridge looking south. The rendering in **Figure 11** shows the proposed concrete pier abutted to the existing in-stream pier, and supporting two precast concrete beams with a cast-in-place concrete deck.



Figure 10: 'Before' photo (left)



Figure 11: 'After' rendering (right) - concrete beams attached to existing bridge, looking under the west side of the bridge

The American Association of State Highway and Transportation Officials (AASHTO) Guide for the Planning, Design, and Operation of Pedestrian Facilities and the AASHTO Guide for the Development of Bicycle Facilities require the pathway on the bridge to be a minimum of 10 ft. of clear width for bike/pedestrian traffic. Additionally, the bridge and the approaches to the bridge would need to meet Americans with Disabilities Act (ADA) requirements.

A bridge with a pathway width of 12 ft. provides greater safety for bicyclists, pedestrians and those with disabilities (e.g. those who use wheel chairs). This would moderately increase the cost of the bridge, but should not significantly change environmental impacts compared to a 10-ft. wide bridge.

Bridge Location Size and Type Summary

The proposed bike/pedestrian bridge is a 240 ft. long three span precast concrete bridge that will match the existing bridge span lengths, abutments and pier locations on the west side of the existing bridge. The foundation at the abutments and piers will likely be driven steel piles. The pathway on the pedestrian bridge will provide a 12 ft. wide pathway measured from the pedestrian rail to rail dimension. The existing concrete bridge parapet will remain as the primary safety barrier between southbound traffic on SH-55 and pathway users on the proposed bike/pedestrian bridge. The pedestrian bridge will tie in with the existing sidewalks and pathway at the northwest corner of the existing bridge, and will tie in with the developing bike/pedestrian path at the southwest corner of the existing bridge. A reinforced earth retaining wall or similar structures will likely be used at the north and south abutments of the new bike/pedestrian bridge. See **Concept Plan** attached to this report.

Design Elements

Because Eagle Road is in a Scenic Overlay District, input should be sought from the Eagle Road Bridge Committee (currently in the process of forming), City of Eagle, and the public during the design process to determine how best to incorporate aesthetically-pleasing colors and materials into the design.

With the help and involvement of members of the Eagle Arts Commission, artistic elements that may be considered include: the abutment wall facing material and texture, new bridge bike/pedestrian rail type and material, and pathway retaining wall material and texture. It is important that each aesthetic element be in harmony with each other and with the existing project site. Materials such as wood, stone, concrete and steel used for aesthetic appeal, must consider durability against environmental degradation and costly maintenance. The current cost estimate reflects a budget of \$15,000 for artistic elements. However, the cost of the aesthetic elements will be dependent on input from the community and Eagle Arts Commission, which may increase the cost for desired aesthetic elements. See attached Foundation Grant List which includes potential funding sources that could be used towards the cost of artistic elements.

Maintenance and Inspection

The recommended concrete bridge will be designed to a 75-year design life, but would likely not need to be replaced for 80 to 100-years. Thus, the proposed bridge will be relatively maintenance-free for the life of the bridge. The proposed bridge is not in a harsh environment

and will not likely experience heavy loads that would overstress the bridge. In the unlikely event that the concrete deck top surface begins to show signs of deterioration such as cracks, pitting or spalls, a concrete sealer may be applied to the top surface after 25 years of service. The cost for sealing the concrete deck in 25 years amounts to approximately \$10,000 (dollars in year 2032). It is more likely that the City will need a maintenance budget for graffiti removal and/or de-icing than sealing the concrete deck. The City should consider incorporating the proposed bridge (once constructed) in its greenbelt/parks system/budget and set aside a minimum of approximately \$500 dollars a year for maintenance.

It has been confirmed with ITD Bridge staff that the proposed pedestrian bridge would be exempt from routine state inspections; therefore, it would be at the City of Eagle's discretion to schedule and/or conduct any bridge inspections.

Project Need

Below is a summary of destinations and attractors, barriers, safety issues, mobility, and economic impacts associated with the proposed project.

Destinations and Attractors

The proposed bike/pedestrian bridge would provide several properties south of the Boise River direct access to Downtown Eagle, the library, Eagle City Hall, parks, multiple schools, retail, shopping, and dining. The proposed bridge would link the community to six popular/regional recreation opportunities, including the Eagle Island State Park, two golf courses, and three completed trails. Within a mile of the proposed bridge, there are two medical service centers, fine and casual dining establishments, two fitness centers and a dance studio, as well as cultural and educational facilities. The proposed bridge connection would also provide critical access to commercial centers directly north of the proposed bridge and to the east of Downtown Eagle, filled with numerous professional offices providing public safety, real estate, accounting and legal services (to name a few). These areas are employment and recreation activity destinations/centers for surrounding residential neighborhoods and the Treasure Valley as a whole. As shown on the attached **Destinations** map, there are numerous planned trail and park connections within the immediate vicinity of the project area that will further connect neighborhoods and to commercial areas to the north and south of the proposed bike/pedestrian bridge. See **Destinations** map attached to this report.

Barriers

Eagle Road/SH-55 is a significant barrier with an average daily traffic of approximately 42,000 vehicles per day, five lanes and 55 mile per hour speed limit. The Boise River creates a divide that makes it difficult for residents to the south to safely access Downtown Eagle and surrounding commercial centers, trails and parks. The bike/pedestrian crossings under the bridge connects over 30 miles of trails on either side of the river, but there's no north-south route over the river to connect these trail segments together. Currently, bicyclists and pedestrians are forced to come up the embankment from the existing trails onto Eagle Road and into fast-paced traffic to cross the Boise River.

Destinations Within Project Area

Schools

- Eagle Middle School
- Eagle Elementary School
- Eagle Hills Elementary School
- Eagle Academy
- Central Academy
- Cecil Andrus Elementary School
- Seven Oaks Elementary School

Parks & Golf Courses

- Boise River Greenbelt
- Eagle Island State Park
- Friendship Park
- Krasen Park
- Arboretum Park
- Heritage Park
- Plaza Street Park
- Merrill Park
- Ban Bury Golf Course
- Eagle Hills Golf Course

Places/Businesses

- Lodging
- Grocery
- Health
- Restaurants & Entertainment
- Services
- Shopping
- Transit stops

Safety

According to ITD crash data, there were a total of 1,091 crashes within the vicinity of the project area between 2010 and 2014. Ten of these crashes were bike related: three A injury (serious injury), six B injury (visible injury), and one C injury (possible injury) crash. Two of these crashes were pedestrian related, both of which were C injury level crashes. Over the five-year time period, crashes increased each year. With a rapidly growing population, bike/pedestrian access is becoming more and more critical. See **Safety Issues** map attached to this report.

Mobility/Connectivity

ADA accessibility is imperative when providing public access. There are no ADA accessible facilities around or onto Eagle Rd to make the connection from north to south over the river. Due to the high speeds on Eagle Rd, this is not a comfortable environment for most users. Vehicular travel is currently the only promotable safe and efficient way to cross over the Boise River. These conditions discourage users from choosing alternate modes of transportation.

The proposed project will facilitate access across the Boise River that has been a long time coming. It will create a critical bike/pedestrian link to a 30+ mile greenbelt system in an area bound by fast-paced highways. The nearest bike/pedestrian river crossing is approximately one-half mile to the east (Merrill Bridge), and the nearest roadway river crossing is over two miles away to the west (Linder Rd). Due to the high cost of a roadway river crossing, bike/pedestrian bridges are the most cost effective and efficient way to accommodate bicycle and pedestrian mobility within the community. Without them, bicyclists and pedestrians would be forced to ride or walk in an unsafe environment, intermixed with high-volume vehicular traffic. Therefore, the proposed will greatly improve mobility and safety for bicyclists and pedestrians in a rapidly growing community.

As shown on the **Pathway Connections** map, the new bridge will enhance circulation to the existing pathway system, as well as planned linkages. Key planned connections within the vicinity include a pathway segment that will ultimately provide direct bike/pedestrian access to Eagle Island State Park to the west, and a segment northwest of the project area that will link bicyclists and pedestrians from State St/SH-44 to Old State St. See **Pathway Connections** map attached to this report.

Economic

Increasing bike/pedestrian access to the existing commercial nodes around the project area has many economic and health benefits. The proposed project will increase recreational and employment opportunities and have a positive impact on the environment. Not only are walking

Growth Within Project Area

2015 Estimate

- 25,653 population
- 9,039 households
- 9,527 jobs

2025 Forecast

- 37,820 population
- 13,690 households
- 11,949 jobs

2040 Forecast

- 45,067 population
- 17,241 households
- 18,753 jobs

Source: COMPASS

and biking more affordable forms of transportation but, in turn, the money saved on automotive transportation will be spent locally at relatively close destinations.

Right-of-Way and Easements

Available assessor records, surveys, record of surveys, topographical data, subdivision plats, and SH-55 bridge plans were reviewed to evaluate established ROW and easements within the proposed project area. There is approximately 90-feet of ROW along the bridge portion of SH-55. There are established public access easements on the west side of the existing bridge to the north where the bike/pedestrian underpass connects to the greenbelt and to the south where the bike/pedestrian underpass connects to an existing dirt detached pathway.

The current proposal for a 14-ft. westerly extension of the existing bridge will leave approximately 23 ft. between the west edge of the new bridge and the westerly ITD ROW line over the river. If more than 23 ft. is needed to construct the new footings, a temporary easement from the Idaho Department of Lands (IDL) and the Mace River Ranch Homeowners Association will likely be needed.

The southerly tie-in, as currently proposed, would be located within the existing ROW and the adjacent property owner, which is the City of Eagle.

The northerly tie-in, as currently proposed, would be within the existing ROW but the end of the return would be located at the northerly edge of the existing IDL 25 ft. wide greenbelt easement. The existing pathways running west and north from the intersection currently fall partially outside the existing IDL greenbelt easement and outside the existing platted pedestrian easement (reserved to the Channel Center Subdivision Property Owner's Association). Access to construct the northerly tie-in may necessitate obtaining a temporary access & construction easement from the owner of Lot 1, Block 1 of Channel Center Subdivision (BB One LLC).

The current cost estimate reflects a budget of \$15,000 for any permanent or temporary construction easements. However, as the project concept progresses further, another evaluation of any potential ROW and/or easement needs should occur. It should be noted that a permit from the IDL for the proposed bike/pedestrian bridge crossing over Boise River will be required. See **Concept Plan** attached to this report (shows property lines, easements and property owners). Note: due to file size, the recorded documents, surveys and plans reviewed have been provided electronically.

Environmental Scan

Project actions that involve a federal nexus (federal funds, federal permits or federal lands) must have an approved environmental document. The Idaho Transportation Department follows Federal Highway Administration (FHWA) guidelines for environmental documentation and is responsible for approval of the environmental document.

An Environmental Screening (**ITD Form 0211**) has been prepared and is attached to this report. The environmental scan has been completed by researching a variety of local, state, and federal maps, records, and database to evaluate environmental resources within the project area and the potential impacts of constructing a bicycle/pedestrian bridge across the North Channel of the Boise River. The environmental scan is not intended to indicate environmental clearance, but to screen for potential environmental issues that may require additional analysis and/or consideration.

Summary - Environmental Resources, Potential Permits, Studies and Consultation

Below is a summary of known potential environmental resources present within the project area, permits, studies, and consultation needs anticipated for the proposed project. See attached **Environmental Screening Form 0211**.

- A permit from Idaho Department of Lands (IDL) to build the bike/pedestrian bridge over the Boise River will be required. IDL administers all public trust lands including navigable rivers in Idaho. The Idaho Department of Water Resources (IDWR) may administer the Stream Alteration Act for encroachments along rivers, but IDL still administers ownership of the public trust lands through issuance of easements and land disclaimers.
- Cultural Resources
 - Consultation with Shoshone Bannock Tribes, Shoshone-Paiute Tribes and Burns Paiute General Council will be required as part of the Cultural Resource process.
 - A qualified Cultural Resource Specialist will need to prepare a Cultural Resources Report to evaluate potential impacts to the Boise River Greenbelt [4(f) resource, potential 6(f)]
- A NPDES Storm Water Discharge Permit will likely be required (if the project will discharge storm water to waters of the U.S. *and* disturb more than one acre of ground)
- USACE Section 404/Stream Alteration Permit
 - Wetland Delineation
 - Water Resources Assessment Report by Qualified Biologist
- A Hydraulics Report will be necessary to evaluate impacts of in-stream work
- A qualified Biologist will need to prepare a Biological Evaluation (BE) to determine if there is “No Effect” to threatened and endangered species and/or critical habitat

- A qualified Biologist will need to assess the Migratory Bird Act and Eagle Nesting Act to provide guidance for minimizing impacts to any known migratory birds within the project area
- A Floodplain Development Permit will be required from the City of Eagle for construction within the floodplain along the Boise River
- City of Eagle Design Review Process Required (project area is within a designated Scenic Overlay District in the City of Eagle. See Feasibility Study for more information on the process overview, timeline, etc.)
- A maintenance agreement between the City of Eagle and ITD will be needed for the proposed bridge structure located within ITD's ROW
- Consultation with emergency officials will be necessary to coordinate maintaining the existing emergency access within the project area

As the project progresses forward, the Environmental Screening form should be updated to reflect any new project or environmental resource information.

Public Involvement Plan

A Public Involvement Plan (PIP) has been developed to provide the framework to the City of Eagle for public involvement efforts recommended to keep stakeholders engaged prior to securing funding and for future implementation of this project. The PIP consists of a list of potential stakeholders, and general guidance on tasks for educating and engaging stakeholders throughout the various phases of the project. Suggested activities include a stakeholder assessment, a bridge committee, news releases, public open houses and ongoing coordination with stakeholders, including established committees, appointed and elected officials. A brief description is provided for each task in the PIP. See **Public Involvement Plan** attached to this report.

To date, stakeholder and public engagement has included but is not limited to:

- Open House/Public Input Meeting led by volunteers (prior to this Project Development effort)
- Two (2) site visits
- Six (6) staff and agency coordination meetings
- Three (3) City Council meetings
- Two (2) Eagle Parks and Pathways Commission meetings

Cost Estimate

Costs to-date invested in the proposed project include approximately \$35,000, with COMPASS Project Development program funds. These funds were used for professional services to develop the feasibility study, and all of the components of this report: project description, project need, ROW analysis, environmental scan, public involvement plan, cost estimate, funding analysis and project schedule.

Once funding is secured, the next step will be to design and construct the proposed project. The Engineer's Estimate of probable cost for design and construction is \$1,299,000 million. Right-of-way/easements, and aesthetic elements may impact the cost. Once a survey is completed and a concept plan is finalized, ROW needs and associated costs will need to be evaluated. The cost for aesthetic/artistic elements will also need to be evaluated as input is provided by the community and Eagle Arts Commission.

As shown in the **Funding** section of this report, it is anticipated that the City of Eagle would apply for multiple grants to help fund this project. **Table 1** includes a summary of the Engineer's Estimate of probable cost for design and construction of the proposed project.

Table 1 – Cost Estimate Summary

Item	Estimated Cost
Design	\$187,000
Right-of-way	\$15,000
Aesthetic features	\$15,000
Construction – bridge structure	\$438,000
Construction – MSE wall on west side of proposed pathway	\$85,000
Construction – pathway connections	\$239,000
Contingency (~20%)	\$156,000
Construction Engineering and Inspection (CE & I)	\$117,000
Administration (by funding agency)	\$47,000
Total	\$1,299,000

See **Project Cost Summary Sheet** (ITD Form 1150) and estimated cost breakdown attached to this report under "ITD Forms".

Funding

It is recommended that the City of Eagle, in partnership with COMPASS, apply for federal and/or state funding sources to fund the design and construction of the Bridge and associated pathway connections. Potential funding sources include but is not limited to:

- **TRANSPORTATION ALTERNATIVES PROGRAM (TAP):** this funding source is applied for and programmed by ITD. Funds could be used for design and construction of the bike/pedestrian bridge. A minimum local match of 7.34 percent would be required.
- **SURFACE TRANSPORTATION PROGRAM-TRANSPORTATION MANAGEMENT AREAS-TRANSPORTATION ALTERNATIVES PROGRAM (STP-TMA-TAP):** this funding source is applied for and programmed by COMPASS. Funds could be used for design and construction of the bike/pedestrian bridge. A minimum local match of 7.34 percent would be required.
- **COMMUNITIES IN MOTION (CIM) IMPLEMENTATION GRANT:** this funding source is managed by COMPASS to provide direct support to member agencies in implementing locally important projects that support the regional goals of the CIM 2040. Applicant agencies will supply a match of at least 7.34 percent of the project cost. In-kind contributions of labor/staff time may be included in the project match. Agencies are encouraged to use this program to leverage local funds and/or federal funds.
- **RECREATIONAL TRAILS PROGRAM (RTP):** this funding source is managed by Idaho Department of Parks and Recreation (IDPR). Funds could be used for trailhead improvements, trail connections, abutments, shared costs, etc. A minimum local match of 20 percent would be required.
- **LAND AND WATER CONSERVATION FUND (LWCF):** this funding source is managed by IDPR. Funds could be used for planning, acquisition and development of needed land and water areas and facilities. A minimum local match of 50 percent would be required.
- **FOUNDATION GRANTS:** there are multiple foundation grants to support bike/pedestrian projects. See **Foundation Grant List** attached to this report.

Partnerships, donations, foundation grants, and local matching dollars are also possibilities for leveraging grant funds. Potential funding scenarios shown in **Tables 2-5** reflect various funding sources and match amounts. The amount of match required to complete the project will depend on which funding sources the City of Eagle is successful in securing. While the match will ultimately be the responsibility of the City of Eagle, additional funds may be sought from other agency partners and private entities to reduce the impact on Eagle's budget.

Table 2 – Scenario 1: Potential Funding Breakdown (TAP + STP-TMA-TAP + CIM + RTP)

Funding Source	Amount
TAP	\$500,000
STP-TMA-TAP	\$500,000
CIM	\$50,000
RTP	\$153,653
Match (7.34%)	\$95,347
Total	\$1,299,000

Table 3 – Scenario 2: Potential Funding Breakdown (TAP + STP-TMA-TAP + CIM)

Funding Source	Amount
TAP	\$500,000
STP-TMA-TAP	\$500,000
CIM	\$50,000
Match (approx. 19-20%)	\$249,000
Total	\$1,299,000

Table 4 – Scenario 3: Potential Funding Breakdown (TAP or STP-TMA-TAP + RTP + CIM)

Funding Source	Amount
TAP or STP-TMA-TAP	\$500,000
CIM	\$50,000
RTP	\$180,000
Match (approx. 43-44%)	\$569,000
Total	\$1,299,000

Table 5 – Scenario 4: Potential Funding Breakdown (TAP or STP-TMA-TAP + CIM)

Funding Source	Amount
TAP or STP-TMA-TAP	\$500,000
CIM	\$50,000
Match (approx. 57-58%)	\$749,000
Total	\$1,299,000

These funding scenarios are hypothetical. Amounts and types of funding may change based on availability of grant funds, local funds, etc.

Project Schedule

The first step will be to develop a Bridge Committee consisting of key stakeholders that will convene following the Project Development phase to provide ongoing dialogue among stakeholders as funding is secured and partnerships are developed for the project. For more information, see the attached Public Involvement Plan (PIP). As shown in the schedule below, it is anticipated that it will take approximately 2 ½ years to complete the project once funding is secured.

Task	Year 1												Year 2												Year 3					
	1	2	3	4	5	6	7	8	9	10	11	12	1	2	3	4	5	6	7	8	9	10	11	12	1	2	3	4	5	6
Funds become available	█																													
Consultant Selection/ Contract Negotiation	█	█	█																											
Notice to Proceed			█	█																										
*Public Involvement			█	█	BC	PR	█	█	█	█	█	BC	PR / OH	█	█	█	█	BC	PR / OH	█	█	█	█	BC	PR	█	█	█	PR	
Survey				█																										
Concept Report				█	█	█	█																							
Environmental Document						█	█	█	█	█	█																			
Preliminary Design								█	█	█	█																			
Design Study Report												█	█																	
Final Design														█	█	█	█													
Utility Plans														█	█															
PS&E																			█	█	█									
Bidding and Contractor Selection																					█	█	█							
Construction																								█	█	█	█	█	█	

Schedule Legend (*)

- BC - Bridge Committee Meeting
- PR - Press Release
- OH - Open House

Next Steps

The following next steps are recommended:

- **BRIDGE COMMITTEE:** Form a bridge committee. A list of potential committee representatives is included in the **Public Involvement Plan** attached to this report. The list of proposed committee members should be presented to the Mayor and City Council for approval. Initial topics may include funding sources and schedules, matching funds, ways to reach out and begin the public input process, etc.
- **FUNDING:** Monitor grant submittal deadlines (i.e. COMPASS, ITD, IDPR, Foundation Grants) closely to allow adequate time to apply for funding. Invite funding representatives to Bridge Committee meetings and schedule site visits, as appropriate, with funding agency representatives. Continue coordination efforts with COMPASS and ITD as the project further develops. Prepare funding applications (City of Eagle), potentially with assistance from COMPASS.
- **AGENCY & PROPERTY OWNER COORDINATION:** Contact IDL to discuss the project and any requirements and/or concerns with crossing the Boise River. Discuss the project with adjacent property owners (Mace River Ranch Subdivision Homeowner's Association and BB One, LLC), to provide awareness about potential easements and/or design coordination efforts.
- **ONGOING PUBLIC INVOLVEMENT/PROJECT AWARENESS:** Provide updates on the City's website regarding the Bridge Committee, status of funding, this Report, etc. Utilize existing email lists, if available, and send updates to residents, local homeowner's associations, and bicycle/pedestrian groups as the project is developed. This early and continuous outreach would be a good way to garner additional support and gain support letters that can be included in funding applications. Regular updates should also be provided to existing bike/ped organizations and committees such as the Foundation for Ada/Canyon Trail Systems (F.A.C.T.S.); Walk and Ride Eagle, LLC; and the COMPASS Active Transportation Work Group.

Agencies and Other Parties Consulted

Agencies

COMPASS

Tom Laws
Associate Planner / Bicycle and Pedestrian
Coordinator
208-475-2240
tlaws@compassidaho.org

Sabrina Minshall, AICP
Director of Planning
208-475-2228
sminshall@compassidaho.org

Mary Ann Waldinger
Principal Planner, Modeling
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mwaldinger@compassidaho.org

City of Eagle

Mike Aho
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208-489-8763, Ext 214
maho@cityofeagle.org

Steve Noyes
Trails Coordinator, Parks & Recreation
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snoyes@cityofeagle.org

Nichoel Baird-Spencer, AICP
Planner III, Planning and Zoning
208-939-0227, Ext 211
nbaird@cityofeagle.org

ITD

Aaron Bauges
District III Project Manager
208-334-8964
Aaron.bauges@itd.idaho.gov

Mike Ebright, P.E.
Group III Design Leader, Bridge Section
208-334-8546
Mike.ebright@itd.idaho.gov

Matt Farrar, P.E.
Chief Bridge Engineer
208-334-8538
Matt.Farrar@itd.idaho.gov

Trii Buu, P.E.
Construction and Materials
208-334-8448
Tri.Buu@itd.idaho.gov

Greg Vitley
ITD District III Environmental Planning
208-334-8952
Greg.Vitley@itd.idaho.gov

Other Involved Parties

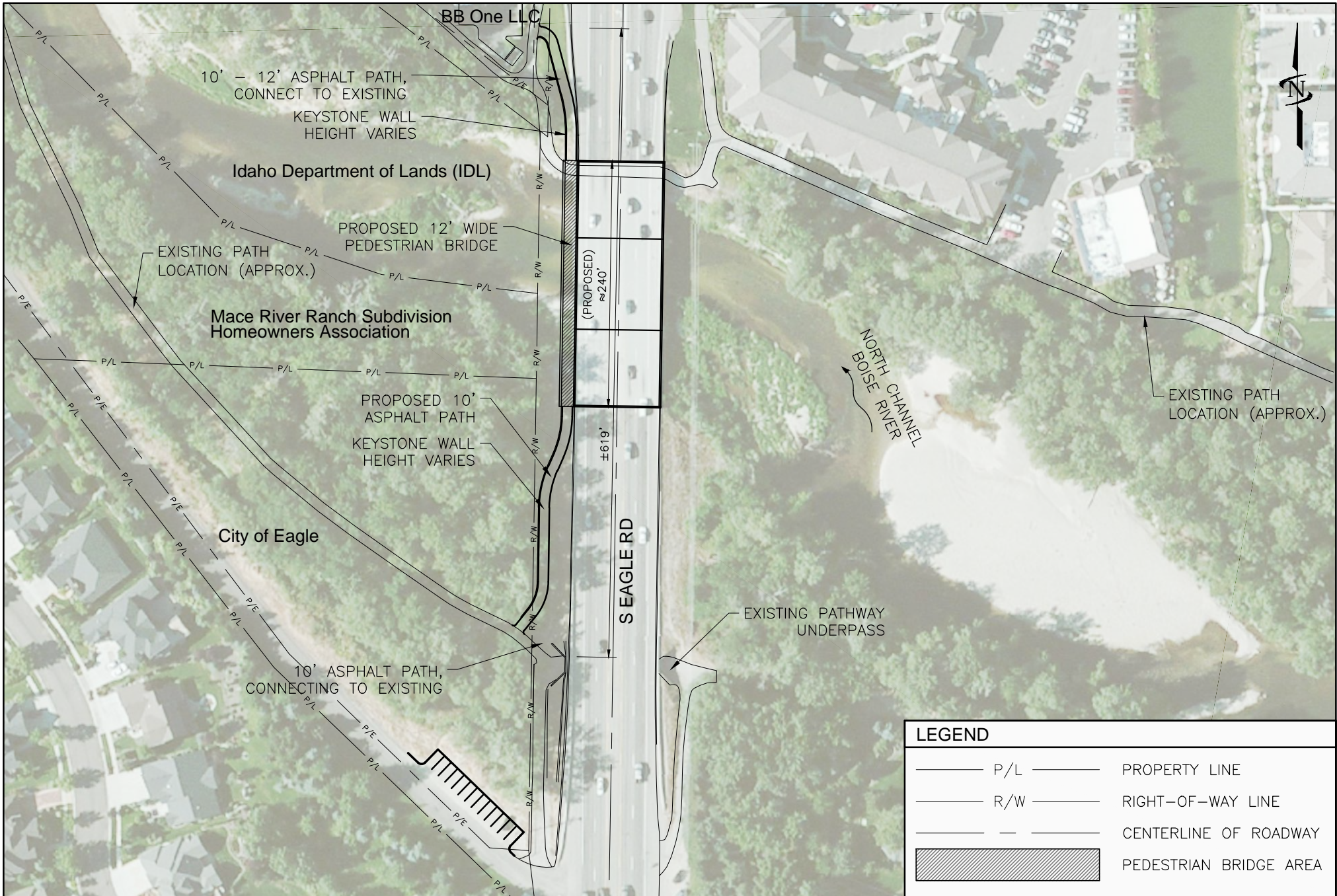
MAADI Group Inc.
Alex de la Chevrotiere
President
450-449-0007 Ext: 225
alex@maadigroup.com

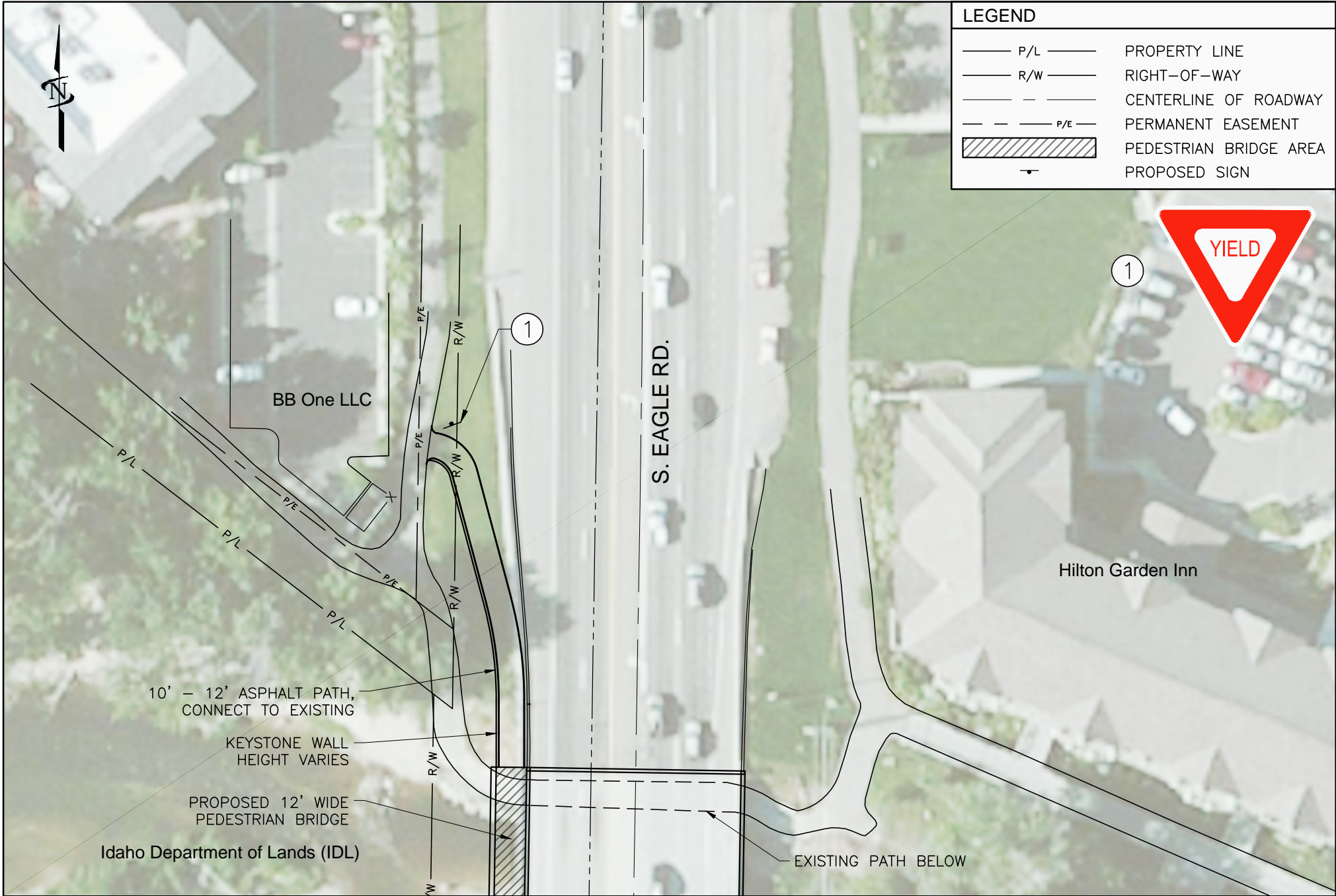
Provided weight of aluminum bridges for 8 feet and 10 feet wide pathway.

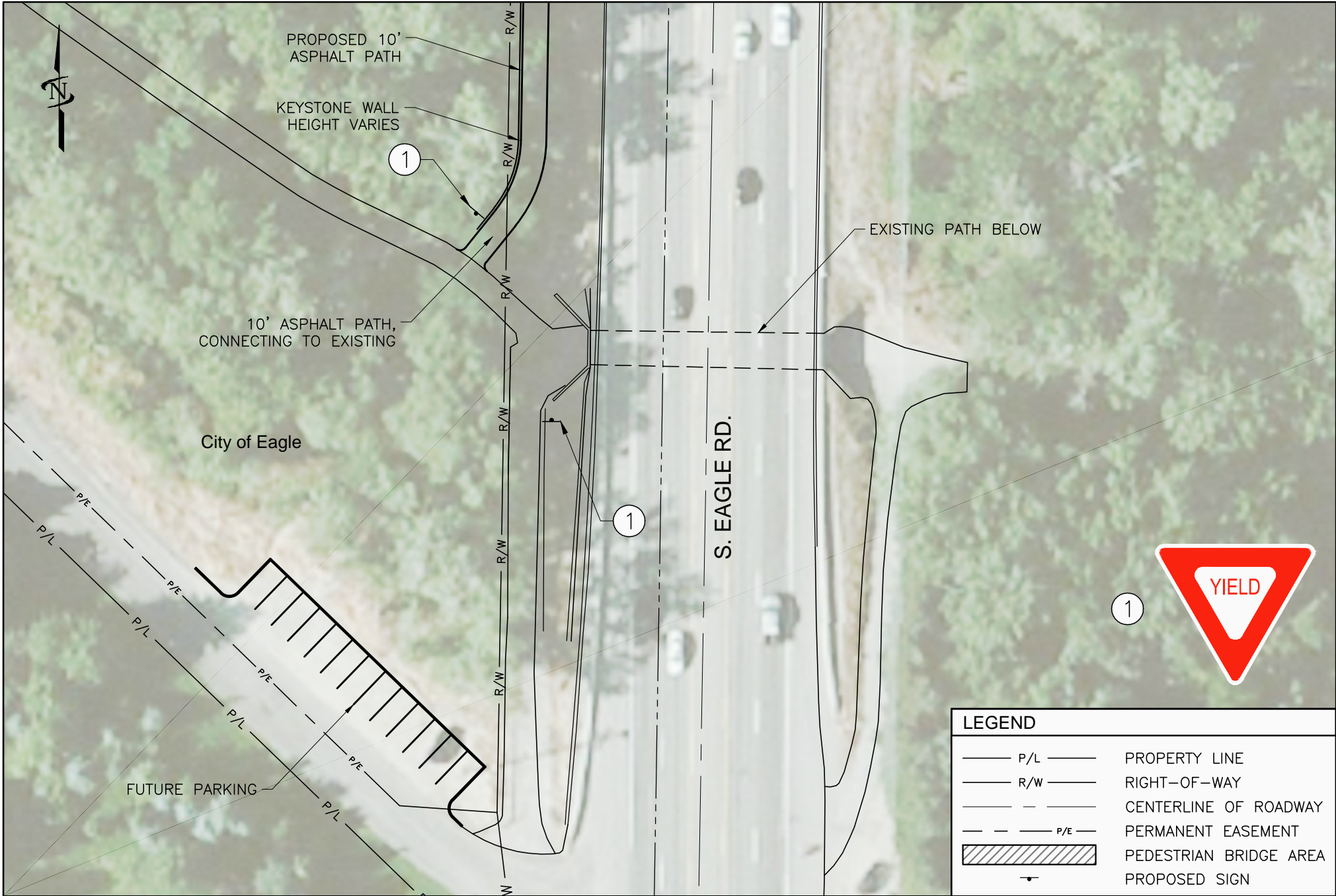
Rick Tholen
Two Rivers; Walk and Ride Eagle, LLC
208-863-5234
f4ister@gmail.com

Initiated the project in the initial/early project stages (aluminum bridge concept)

Concept Plans and Renderings







EAGLE RD BICYCLE AND PEDESTRIAN BRIDGE
EAGLE, IDAHO

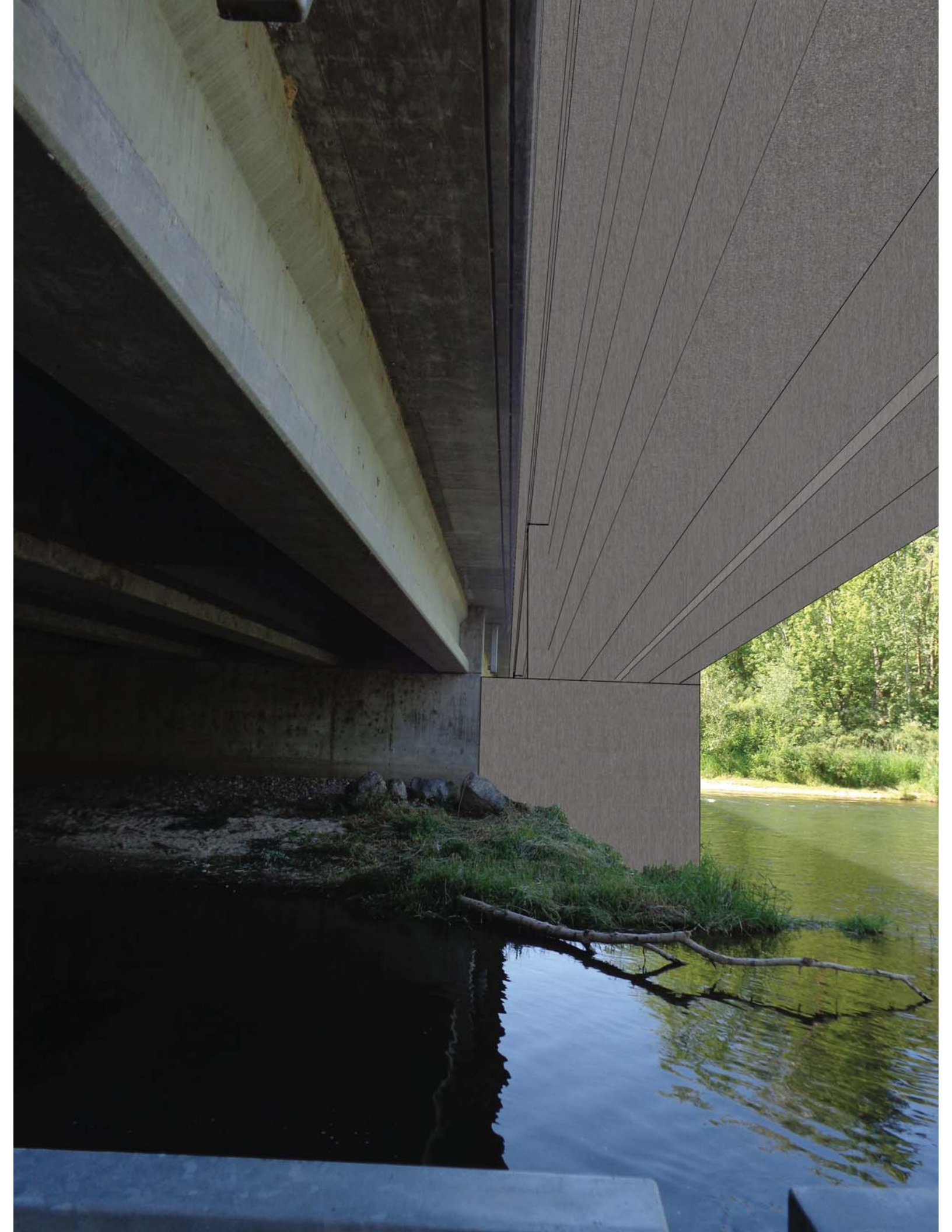


S.W. CORNER CONCEPT PLAN

MARCH 2016



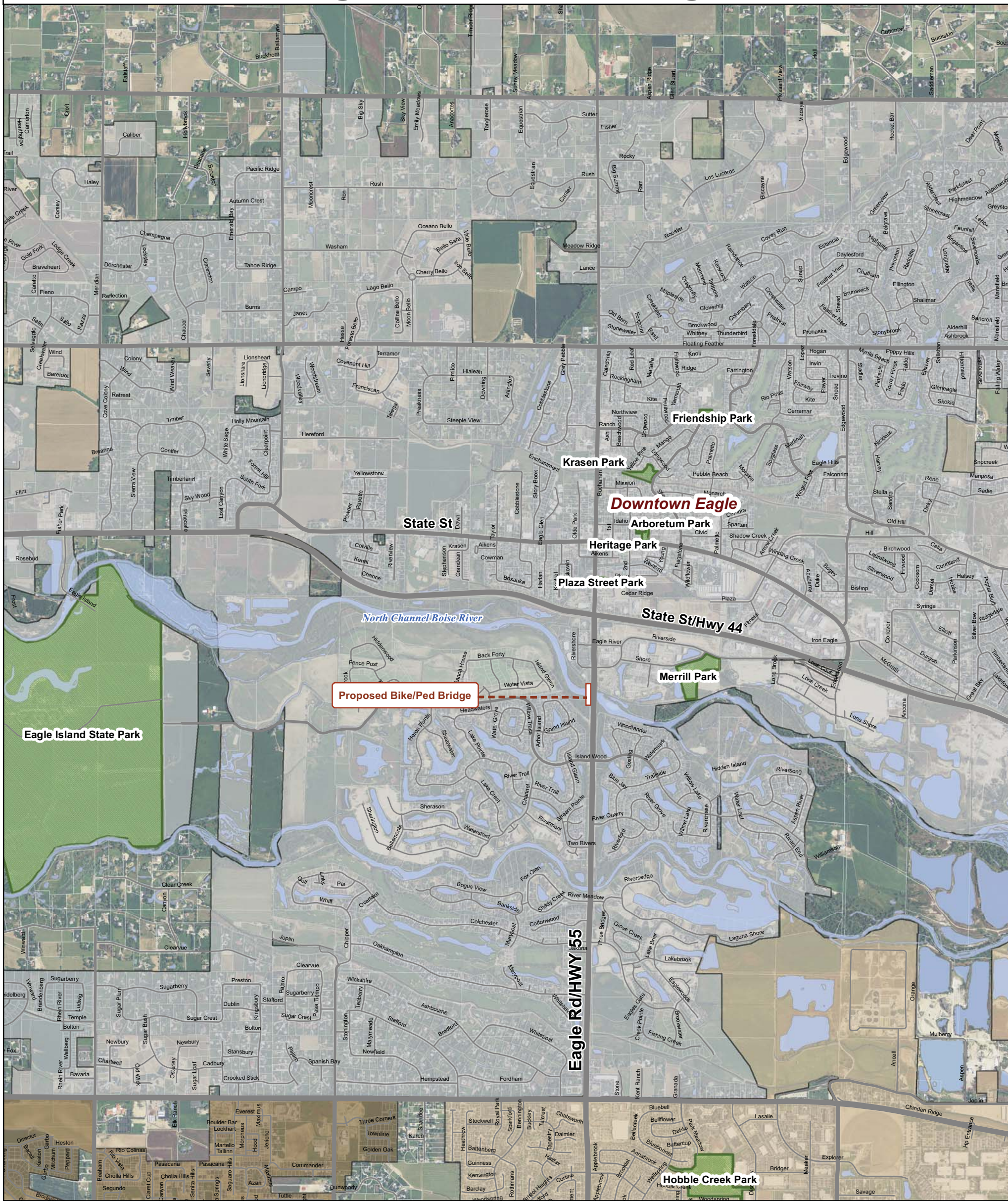
YIELD TO PEDS







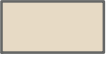




Maps

VICINITY MAP

Eagle Road Bike/Ped Bridge



LEGEND

- | | | |
|---|--|--|
| Cities | Roads |  Water Bodies |
|  Eagle |  Principal Arterial |  Parks |
|  Boise |  Minor Arterial | |
|  Meridian |  Collector | |
| |  Local | |

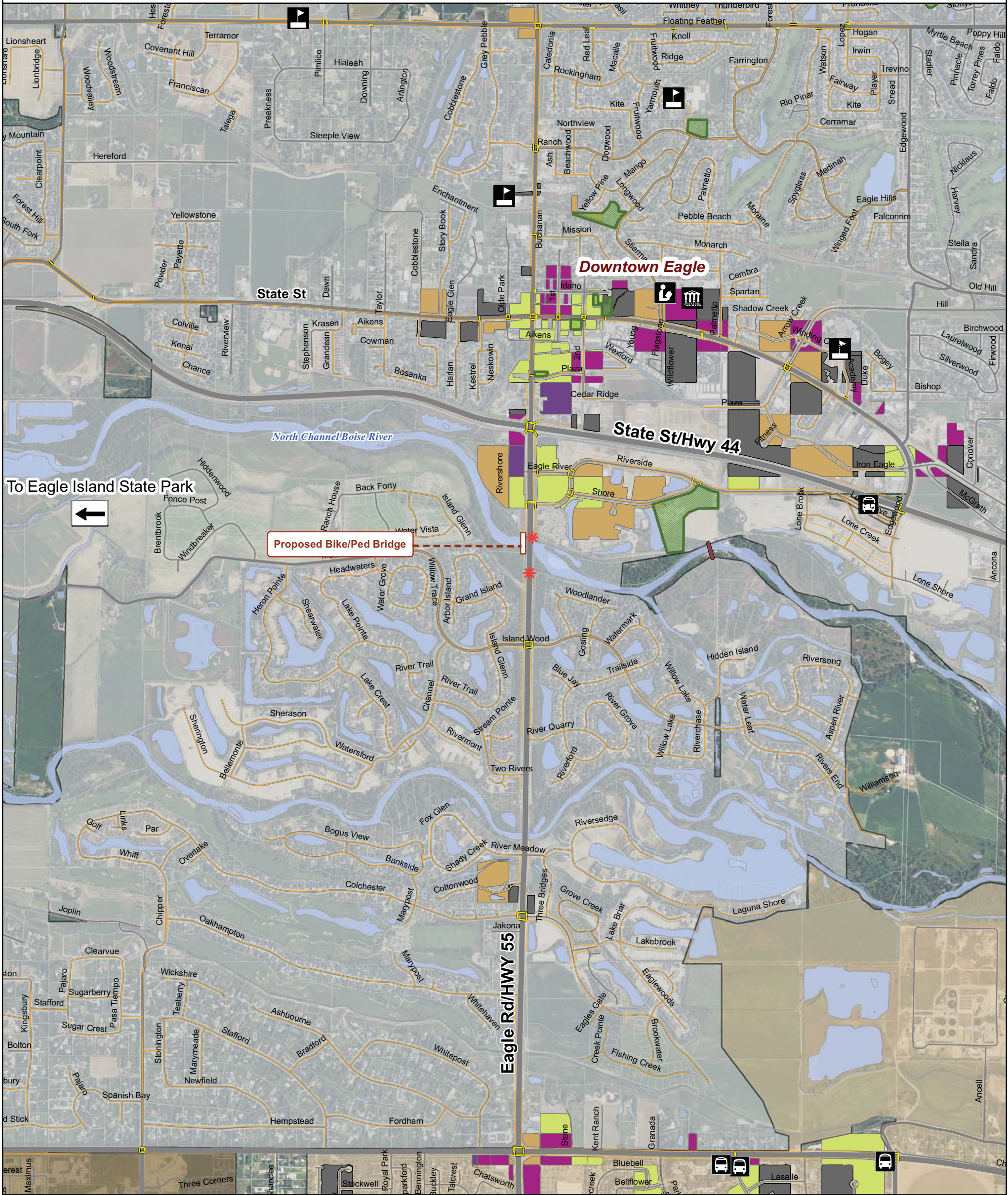
0 1,000 2,000 4,000 Feet

FIGURE 1.1
VICINITY MAP



DESTINATIONS

Eagle Road Bike/Ped Bridge



LEGEND

Places

- City Hall
- Library
- School
- Transit Stops

Destinations

- Grocery
- Health
- Parks
- Resturant/Entertainment
- Services
- Shopping

Cities

- Eagle
- Boise
- Meridian

Existing Bike/Ped Facilities

- Bike/Ped Bridge
- Crosswalk
- Sidewalk
- Pathway Underpass

FIGURE 1.2
DESTINATIONS



0 650 1,300 2,600
Feet

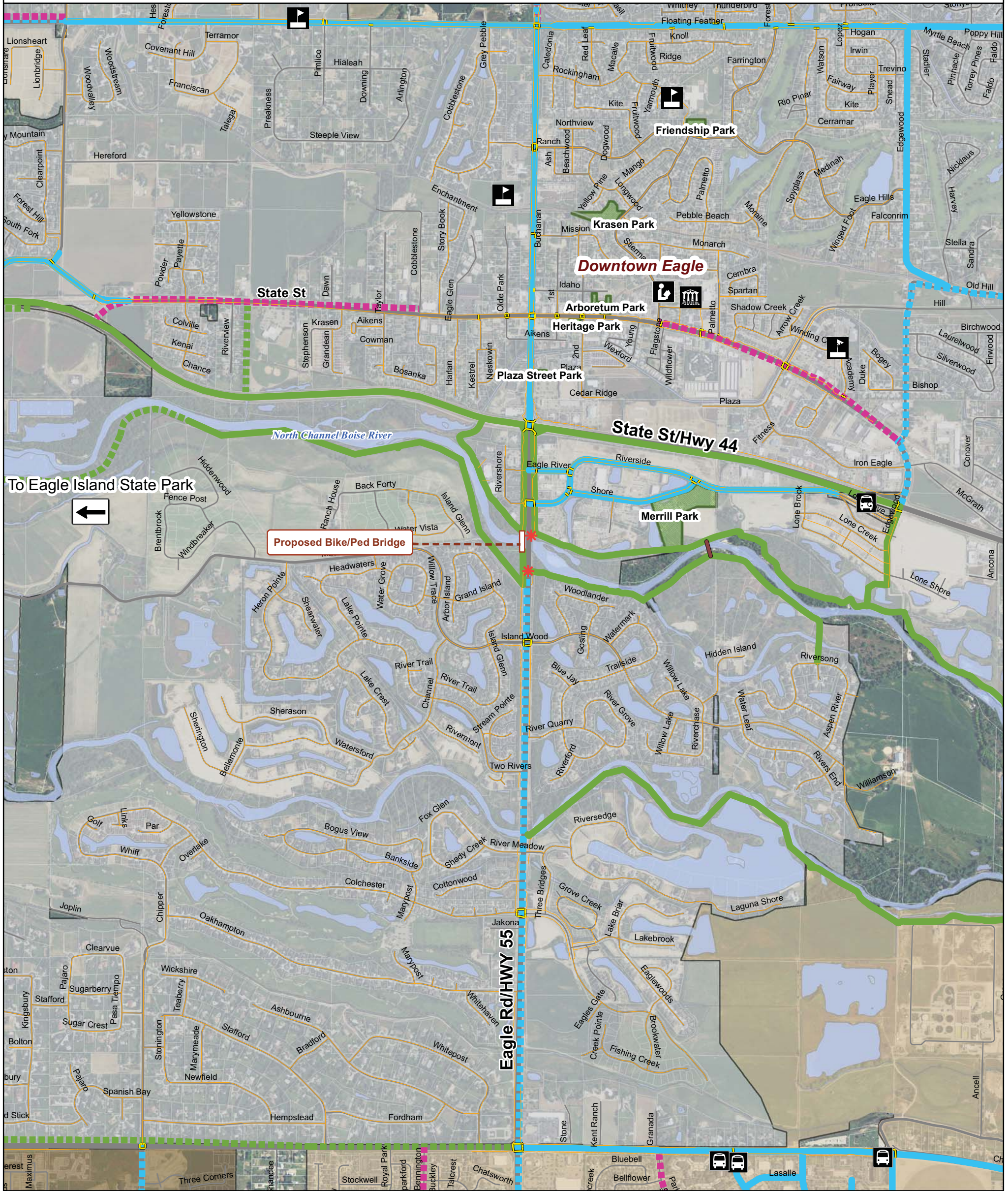
* Data Provided by COMPASS



J-U-B ENGINEERS, INC.

PATHWAY CONNECTIONS

Eagle Road Bike/Ped Bridge



LEGEND

Places	Cities	Existing Bike/Ped Network	Future Bike/Ped Network
City Hall	Eagle	Existing Bike Lane	Future Bike Lane
Library	Boise	Existing Bike Route	Future Bike Route
School	Meridian	Existing Multi-Use Path	Future Multi-Use Path
Transit Stops		Bike/Ped Bridge	
Parks		Crosswalk	
		Sidewalk	
		Pathway Underpass	

0 650 1,300 2,600 Feet

FIGURE 1.3

PATHWAY CONNECTIONS

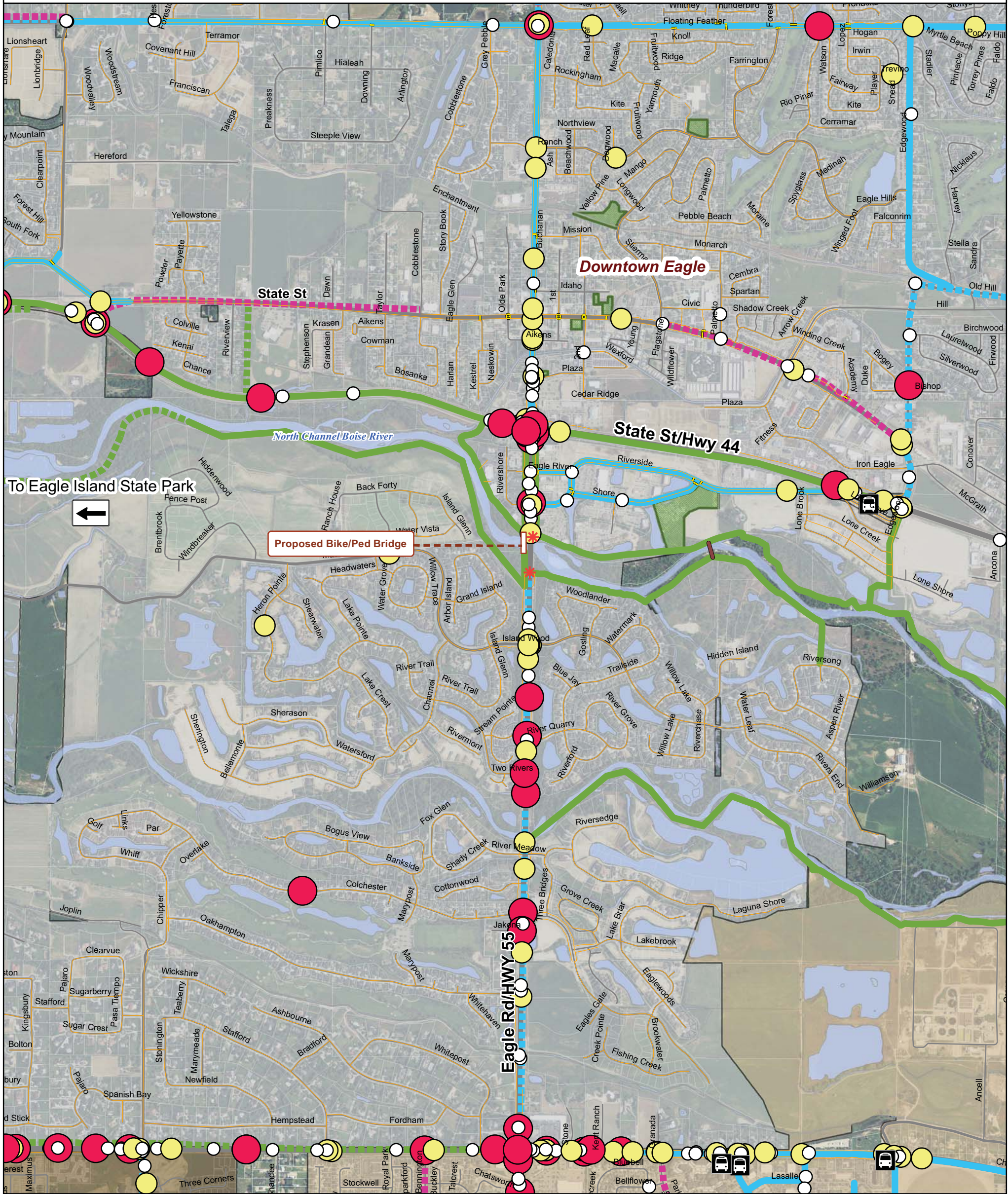


* Data Provided by COMPASS

J-U-B ENGINEERS, INC.

SAFETY ISSUES

Eagle Road Bike/Ped Bridge



LEGEND

- Crashes (2010-2014)**
- A Injury Accident
 - B Injury Accident
 - C Injury Accident

- Cities**
- Eagle
 - Boise
 - Meridian

- Existing Bike/Ped Network**
- Existing Bike Lane
 - Existing Bike Route
 - Existing Multi-Use Path
 - Bike/Ped Bridge
 - Crosswalk
 - Sidewalk
 - ✱ Pathway Underpass
 - Parks
 - Transit Stops

- Future Bike/Ped Network**
- Future Bike Lane
 - Future Bike Route
 - Future Multi-Use Path

0 650 1,300 2,600 Feet

FIGURE 1.4
SAFETY ISSUES



J-U-B ENGINEERS, INC.

* Data Provided by ITD

Photo Exhibits

Photo Exhibit

Eagle Road Bike/Ped Bridge



Photo 1 (Left): Standing on east side of existing Eagle Rd/SH-55 bridge, looking north/west. Pedestrians walking on shoulder as 55 MPH vehicular traffic passes by.

Photo 2 (Right): Standing on west side of existing Eagle Rd/SH-55 bridge, looking towards the north abutment. The proposed bike/ped bridge would be adjacent to the existing bridge.





Photo 3 (Above): Existing Eagle Rd/SH-55 bridge, looking south/east at bike/ped underpass.

Photo 4 (Below): Rendering in photo showing proposed bike/ped bridge adjacent to the existing bridge.





Photo 5 (Left): Existing Eagle Rd/SH-55 bridge, west side, looking south.



Photo 6 (Right): Rendering in photo showing proposed bike/ped bridge adjacent to existing bridge.

Public Involvement Plan

Bike/Ped Bridge over North Channel of Boise River; ID 55/Eagle Road

PUBLIC INVOLVEMENT PLAN

INTRODUCTION

The public information and education process is characterized by communication with stakeholders that is early, continuous, meaningful, and inclusive throughout the life of the project. This Public Involvement Plan (PIP) is a “living document” that can be used by the City of Eagle as the project moves forward.

COMPASS has retained J-U-B Engineers to assist with the “project development” phase of a project that would attach a bike/pedestrian bridge adjacent to the existing SH-55 Bridge. This PIP is meant to provide the framework that the City of Eagle can use to keep stakeholders engaged prior to and during securing funding, and for future implementation of this project.

PUBLIC INFORMATION GOALS

Public involvement activities should be designed to inform, educate and solicit input from project area stakeholders regarding the Eagle Road Bike/Ped Bridge project. In addition to public education efforts, proper support for stakeholder communications to ensure questions and concerns are addressed in a timely manner will be vital to the success of the project.

POTENTIAL STAKEHOLDERS

The following list is not inclusive and will likely change and grow throughout the process.

- Ada County
- Ada County Highway District (ACHD)
- Adjacent Property Owners
- Army Corps of Engineers
- Development Services Inc.
- Eagle Arts Commission
- Eagle City Council
- Eagle Fire Department
- Eagle Parks and Pathways Development Commission
- Eagle Transportation Committee
- Eagle Urban Renewal District
- Foundation for Ada/Canyon Trail Systems (FACTS)
- Gardner Company
- Idaho Department of Lands (IDL)
- Idaho Department of Parks and Recreation (IDPR)
- Idaho Transportation Department (ITD)
- Law enforcement
- Local Business Owners (Island Woods, Laguna)
- Shoshone Bannock Tribe
- State Historic Preservation Commission
- Two River Community
- Walk and Ride Eagle, LLC

SITUATION ASSESSMENT/KEY STAKEHOLDER INTERVIEWS

An early situation assessment would help the City of Eagle identify key stakeholders and gain a better understanding of their issues, needs and perceptions as it relates to the bridge. Early contact with stakeholders would also help educate the community on the project’s purpose and schedule.

Upon completion of key stakeholder contacts, critical information shared should be documented to inform decision makers of the public's interests, as well as guide future public involvement efforts.

BRIDGE COMMITTEE

It is recommended that the City of Eagle develop a Bridge Committee consisting of key stakeholders following the Project Development phase to provide ongoing dialogue among stakeholders as funding is secured and partnerships are developed for the project. The Bridge Committee should meet on a regular basis in order to provide critical information to stakeholders, receive valuable input from a variety of perspectives, and allow an opportunity to build increased public support for the project. Once funding is secured, the Bridge Committee should meet at least three times: at the beginning of the project to evaluate plans and collect feedback on public interests and concerns; prior to completing project plans to gain agreement and support for moving forward; and a final meeting to identify and address any impediments or new opportunities that arise during the final stages of the project. Additional meetings may be necessary depending on the level of public concern and engagement shown for the project.

NEWS RELEASES

It is recommended that news releases providing information on the project, including purpose, funding and schedule, be drafted and provided to local media outlets at milestone points throughout the project, such as at the beginning of the development phase and when the project nears completion. It is recommended that point person be designated for all media inquiries to ensure that messaging around the project is accurate and consistent.

PUBLIC OPEN HOUSES

It is recommended to conduct a minimum of two public open houses to allow the community opportunities to provide input on the project and discuss issues, concerns and potential recommendations at the beginning of the project and prior to finalization of design plans. Advertisement and/or legal notice for the meeting should be sent out prior to the open house alerting the public of the time and location of the meeting. A summary of citizen comments should also be developed for future use.

STAKEHOLDER COORDINATION

It is recommended that the City respond to stakeholder inquiries and maintain communication through personal visits, phone and/or emails in order to answer questions and ensure consistency in message. In addition, a database of stakeholders can be developed and maintained in order to communicate updates and track concerns, issues and interactions.

PUBLIC OFFICIAL COORDINATION

It is recommended that regular updates be provided to elected and appointed public officials (Eagle City Council, Eagle Parks and Pathways Development Commission) to coordinate funding efforts, share public input and obtain input as the project progresses.

Foundation Grant List

Foundation Grant Opportunities

Art & Bike/Pedestrian Projects

Updated April 2016

Note: This list is meant to serve as a general guide. It does not constitute all funding opportunities available, nor does it guarantee eligibility for a particular program.

Funding Program	Program Information	Eligibility Criteria	Application Date	Maximum Grant	Minimum Match	Notes	Link
National Endowment for the Arts: Our Town Grant	Art work to support creative, economically-competitive, healthy, resilient, and opportunity-rich communities.	Partnerships that involve two primary partners: a nonprofit organization and a local governmental entity (one of the two primary partners must be a arts or design organization)	December 15, 2014	Request a grant amount at one of the following levels: \$25,000, \$50,000, \$75,000, \$100,000, \$150,000, or \$200,000	1:1	Prior to applying for this grant, it is recommended to call the funding staff and follow their advice. It may be necessary to involve the Eagle arts committee to help define the project prior to applying for funds.	http://arts.gov/grants-organizations/our-town/introduction
National Endowment for the Arts: Art Works	Support the creation of art that meets the highest standards of excellence, public engagement with diverse and excellent art, lifelong learning in the arts, and the strengthening of communities through the arts.	Nonprofit, tax-exempt 501(c)(3), U.S. organizations; units of state or local government; or federally recognized tribal communities or tribes may apply. Applicants may be arts organizations, local arts agencies, arts service organizations, local education agencies (school districts), and other organizations that can help advance the goals of the National Endowment for the Arts.	Deadline: October 20, 2015 Notification: April 2016 Earliest Start Date: May 1, 2016	Matching grants generally range from \$10,000 to \$100,000.	A minimum cost share/match equal to the grant amount is required.	Contact Info: https://www.arts.gov/grants/apply-grant/grants-organizations/agency-contacts	https://www.arts.gov/grants-organizations/art-works/grant-program-description
Artplace America	Creative place-making: strengthening the social, physical, and economic fabric of a community through arts and culture. It is best to integrate with a community's economic development and revitalization strategies.	Non-profit organizations, local governing bodies, individual artists/designers, and for-profit organizations within the U.S. and all U.S. Territories.	October webinars November 3, 2014 Letter of Inquiry Deadline. January 2015 – applicants notified, requesting full proposal. March 2015 – deadline for submitting full proposal.	\$50,000-\$500,000 \$10 million total in FY2015	Not required but encouraged.	Prior to applying for this grant, it is recommended to call the funding staff.	http://www.artplaceamerica.org/oi/national-grants-summary/
Laura Moore Cunningham Foundation	Grant-making priorities include rural healthcare, educational programs for children, and programs in underserved communities and for underserved populations. The Foundation does not limit giving to a certain type of program or need (i.e. programs, equipment, bricks and mortar, salaries and benefits), however administrative costs are not preferred. We are interested in organizations run in a cost-effective manner and serving large numbers of people who are truly in need.	The Foundation accepts applications from any qualified tax-exempt Idaho nonprofit organization.	<ul style="list-style-type: none"> • Applications must be postmarked by May 15. • Grants will be announced and disbursed by August 31. 	\$1,000 - \$500,000 (no specific amount – these amounts reflect past reward amounts)	Unknown	List of 2015-funded projects: http://nebula.wsimg.com/4e3d9e11b82c1e684354603fbae3cad7?AccessKeyId=0501A1F89A0D652586A4&disposition=0&alloworig in=1	http://www.lauramoorecunningham.org/grants.html

Funding Program	Program Information	Eligibility Criteria	Application Date	Maximum Grant	Minimum Match	Notes	Link
Petroglyph Energy Foundation	<p>Projects that support volunteer and financial investment to make communities healthier and better places to live. Adequate health and welfare services, good education facilities and well-managed civic, economic and cultural organizations enhance the development, prosperity and quality of life of a community.</p> <ul style="list-style-type: none"> • Health and Human Services • Education • Civic and Community • Arts and Culture 	Unknown	Unknown	Unknown	Unknown	Contact Info: http://www.intermountainindustries.com/contact.php	http://www.intermountainindustries.com/community.php
Idaho State Historical Society (SHPO) Certified Local Government (CLG) Program	<p>Local governments that wish to participate must meet the following minimum requirements:</p> <ul style="list-style-type: none"> • Establish by state law or local ordinance an adequate and qualified historic preservation review commission consisting of professional and lay members. • Conduct a survey and maintain an inventory of historic properties in the community. • Provide for adequate public participation in the historic preservation program, including the process for nominating properties to the National Register of Historic Places. • Encourage local historic preservation planning efforts. • Enforce appropriate state and local legislation for the designation and protection of historic properties. <p>Maintain a satisfactory ongoing performance of these duties.</p>	Cities, counties and tribes are eligible applicants.	Check with agency; typically receive notice within 30 days of application submittal.	Recommend \$2,000	50% local match share of the total project cost	Idaho SHPO typically distributes about \$70,000 annually to city and county programs through the CLG program. Call 208-334-3847 to discuss projects, eligibility, etc.	http://history.idaho.gov/certified-local-government-clg-program
Simplot Foundation	<ul style="list-style-type: none"> • Arts • Education • Youth • Community • Industry 	501(c)3 organizations	Unknown	Unknown	Unknown	Prior to applying for this grant, it is recommended to call the funding staff.	http://www.simplot.com/community_involvement
J.A. and Kathryn Albertson Foundation	<ul style="list-style-type: none"> • Awareness • Community Investments • Learning Innovation 	Non-profit, government agencies (need to confirm)	Unknown	Unknown	Unknown	Prior to applying for this grant, it is recommended to call the funding staff. Contact names: http://www.jkaf.org/about/	http://www.jkaf.org/

Funding Program	Program Information	Eligibility Criteria	Application Date	Maximum Grant	Minimum Match	Notes	Link
Idaho Commission on the Arts Grant	<p>Arts education grants, folk and traditional arts, organizational, individual.</p> <p>The Commission uses the following basic criteria to measure the merits of most applications. Each grant or award will contain important, specialized considerations:</p> <ul style="list-style-type: none"> • High artistic quality or artistic merit. • Sound governance, management, and operations. • Financial soundness with evidence of wide support. • Extensive public benefit, community interest, and access for underserved populations, including older people and people with disabilities. • To the extent possible, an organization's project should be part of a long-term cultural plan that outlasts the grant. <p>Commitment to arts education and to promoting understanding and public awareness of the arts.</p>	<p>Most grants and awards are made on an annual basis (except for QuickFunds) and new applications must be submitted each year. Grant and award programs are highly competitive and eligibility or a current grant does not guarantee future funding.</p>	<p>December-March (varies-call to verify)</p>	<p>Recommend \$2,000-\$7,000</p>	<p>1:1 – 1:3 (varies based on eligible activity)</p>	<p>Set up an appointment to discuss project with program representatives prior to submitting an application.</p>	<p>http://arts.idaho.gov/grants/overview.aspx</p>
People for Bikes (formerly known as Bikes Belong)	<p>PeopleForBikes focuses most grant funds on bicycle infrastructure projects such as:</p> <ul style="list-style-type: none"> • Bike paths, lanes, trails, and bridges • Mountain bike facilities • Bike parks and pump tracks • BMX facilities • End-of-trip facilities such as bike racks, bike parking, and bike storage <p>They also have funded some advocacy projects, such as:</p> <ul style="list-style-type: none"> • Programs that transform city streets, such as Ciclovías or Open Streets Days • Initiatives designed to increase ridership or the investment in bicycle infrastructure 	<p>PeopleForBikes accepts grant applications from non-profit organizations with a focus on bicycling, active transportation, or community development, from city or county agencies or departments, and from state or federal agencies working locally.</p>	<p><u>Fall 2016 Grant Cycle</u></p> <ul style="list-style-type: none"> • Online application opens: June 13, 2016 • Online Letter of Interest due: July 29, 2016 • Notification of LOI status: September 2, 2016 • Full Applications due: October 7, 2016 • Grant award notifications: December 2, 2016 	<p>Up to \$10,000</p>	<p>Unknown</p>	<p>Contact Info: Zoe Kircos, Grant Manager, 303-449-4893 x106 zoe@peopleforbikes.org</p> <p>PeopleForBikes only funds projects in the United States. Requests must support a specific project or program; they do not grant funds for general operating costs.</p>	<p>http://www.peopleforbikes.org/pages/community-grants</p>

Funding Program	Program Information	Eligibility Criteria	Application Date	Maximum Grant	Minimum Match	Notes	Link
New Mobility West	An initiative that provides communities across the Rocky Mountain West with tools and resources to improve their transportation systems while creating safer and more connected neighborhoods, stronger and more vibrant downtowns and a healthier economy.	Applications can come from municipal or county governments, downtown development authorities, urban renewal authorities or non-profit organizations. However, strong partnerships are essential for moving a project forward. This is why in addition to a project narrative, it is required that applicants submit letters of support from project partners and key stakeholders.	Applications for the Spring/Summer 2016 round of assistance under NMW's Community Assistance Program are being accepted through Friday, March 18th, 2015.	Unknown	Unknown	Contact Info: Jillian Sutherland, AICP Program Director 817 Colorado Ave Ste 200 Glenwood Springs, CO 81601 970.928.3411 jillian@communitybuilders.org	http://newmobilitywest.org/
Lowe's Charitable and Educational Foundation Grant	Founded in 1957, Lowe's Charitable and Educational Foundation has a long and proud history of improving the communities we serve. The foundation's support has grown to match the growing needs of communities, going from \$3 million in contributions in 2004 to nearly \$19 million in 2010. The foundation's primary philanthropic focus centers on K-12 public education and community improvement. Within these areas, Lowe's Foundation is committed to supporting projects that have the greatest impact on our communities and align with our core business — home improvement.	Lowe's Charitable and Educational Foundation funds nonprofit organizations and public agencies that support their charitable goals.	Unknown	Recommended \$2,500 - \$10,000	Unknown	Contact local Lowe's for contact info.	http://www.lowes.com/cd_Corporate+Citizenship_674540029 Note: information is dated.
The Home Depot Community Impact Grants Program	Grants must support work completed by community volunteers in the U.S. Projects must be completed within six months following notification that the grant has been awarded.	Only IRS-registered 501c designated organizations and tax-exempt public service agencies (e.g. Police/Fire Departments) in the U.S. are eligible to apply. In very limited circumstances, applications that are submitted by organizations that do not meet this guideline will be considered, but only to the extent that they are requesting funds to support a charitable purpose as defined by the IRS.	Now accepting applications.	Grants, up to \$5,000, are available to IRS-registered 501c designated organizations and tax-exempt public service agencies in the U.S. that are using the power of volunteers to improve the physical health of their community. Grants are given in the form of The Home Depot gift cards for the purchase of tools, materials, or services.	Unknown	Grants are solely given in the form of The Home Depot gift cards for the purchase of tools, materials, or services. Organizations who have received funding through The Home Depot Foundation's Community Impact Grant Program must wait 12 months after notification of award before applying for additional grants through this program.	http://homedepotfoundation.org/page/applying-for-a-grant
Idaho Community Foundation Grant: ICF Regional Competitive Grant Cycle	To enrich the quality of life throughout Idaho; grants include arts and culture, conservation/ environment, education, emergency services, libraries, public projects, recreation, and social services.	Government entities (including subsidiaries and public educational institutions), and entities with current 501(c)3 status.	Opens May 1, closes July 1	Up to \$5,000	Not required	This grant could fund a variety of projects. Ideas include: parking lot upgrades, ADA access to public and cultural facilities, improved access to the library, etc.	http://idcomfdn.org/Regional_Cycle

Funding Program	Program Information	Eligibility Criteria	Application Date	Maximum Grant	Minimum Match	Notes	Link
Capital Matrix: The Jeff Tunison Community Fund	Supports economic impact, strategic importance, quality of the project, potential job creation or retention, and needs of the community.	Governmental entities and non-profit corporations located in the following counties: Ada, Adams, Boise, Canyon, Elmore, Gem, Owyhee, Payette, Valley and Washington.	December 12, 2014; Grant recipients announced March 2015	FY 15 total \$120,000	Not required	Jim Birdsall: (208) 859-0730; email: jbirdsall@cablone.net .	http://www.capitalmatrix.org/GrowingCommunities.aspx

For other bike/ped funding opportunities and programs, click on this link: http://itd.idaho.gov/bike_ped/Info/Related%20Programs%20and%20Project%20Funding.pdf

ITD Forms

Local Federal-Aid Project Request



Instructions

1. Under Character of Proposed Work, mark appropriate boxes when work includes Bridge Approaches in addition to a Bridge.
2. Attach a Vicinity Map showing the extent of the project limits.
3. Attach an ITD 1150, Project Cost Summary Sheet.
4. Signature of an appropriate local official is the only kind recognized.

Note: In Applying for a Federal-Aid Project, You are Agreeing to Follow all of the Federal Requirements Which Can Add Substantial Time and Costs to the Development of the Project.

Sponsor (City, County, Highway District, State/Federal Agency) City of Eagle, Idaho			Date 3/17/2016		
Project Title (Name of Street or Road) Bicycle and Pedestrian Bridge over the North Channel of the Boise River		F.A. Route Number 27	Project Length 240 feet	Bridge Length 240 feet	
Project Limits (Local Landmarks at Each End of the Project) West side of the existing Eagle Road/State Highway 55 (SH 55) Bridge (the bridge) over the north channel of the Boise River					
Character of Proposed Work (Mark Appropriate Items)					
<input type="checkbox"/> Excavation	<input checked="" type="checkbox"/> Bicycle Facilities	<input type="checkbox"/> Utilities	<input type="checkbox"/> Sidewalk		
<input type="checkbox"/> Drainage	<input type="checkbox"/> Traffic Control	<input type="checkbox"/> Landscaping	<input type="checkbox"/> Seal Coat		
<input type="checkbox"/> Base	<input checked="" type="checkbox"/> Bridge(s)	<input type="checkbox"/> Guardrail	<input checked="" type="checkbox"/> Bike/Ped Bridge		
<input type="checkbox"/> Bit. Surface	<input type="checkbox"/> Curb & Gutter	<input type="checkbox"/> Lighting			
Estimated Costs (Attach ITD 1150, Project Cost Summary Sheet)					
Preliminary Engineering (ITD 1150, Line 1)		\$ 234,000			
Right-of-Way (ITD 1150, Line 2)		\$ 15,000			
Construction (ITD 1150, Line 18)		\$ 1,050,000			
Preliminary Engineering By: <input type="checkbox"/> Sponsor Forces <input checked="" type="checkbox"/> Consultant					
Checklist (Provide Names, Locations, and Type of Facilities)					
Railroad Crossing		N/A			
Within 2 miles of an Airport		N/A			
Parks (City, County, State or Federal)					
Environmentally Sensitive Areas		Boise River and Greenbelt			
Federal Lands (Indian, BLM, etc.)					
Historical Sites		8 Total Historic Sites in the City of Eagle (outside project area)			
Schools		Eagle Elementary, Middle and High Schools (outside project area)			
Other					
Additional Right-of-Way Required: <input type="checkbox"/> None <input checked="" type="checkbox"/> Minor (1-3 Parcels) <input type="checkbox"/> Extensive (4 or More Parcels)					
Will any Person or Business be Displaced: <input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> Possibly					

Standards	Existing	Proposed	Standards	Existing	Proposed
Number of Lanes			Roadway Width (Shoulder to Shoulder)	ft	ft
Pavement Type			Right-of-Way Width	ft	ft

Sponsor's Signature	Title
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Additional Information to be Furnished by the District

Functional Classification	Terrain Type	20	ADT/DHV
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Project Cost Summary Sheet

ITD 1150 (Rev. 09-13)
itd.idaho.gov

Round Estimates to Nearest \$1,000

Key Number	Project Number			Date
				3/17/2016
Location				District
Eagle Road Bike/Pedestrian Bridge				3
Segment Code	Begin Mile Post	End Mile Post	Length in Miles	
002005	17.64	17.76	0.12	

	Previous ITD 1150	Initial or Revise To
1a. Preliminary Engineering (PE)		\$47,000
1b. Preliminary Engineering by Consultant (PEC)		\$187,000
2. Right-of-Way: Number of Parcels Number of Relocations		\$15,000
3. Utility Adjustments: <input type="checkbox"/> Work <input type="checkbox"/> Materials <input type="checkbox"/> By State <input type="checkbox"/> By Others		
4. Earthwork		\$39,000
5. Drainage and Minor Structures		
6. Pavement and Base		\$20,000
7. Railroad Crossing:		
Grade/Separation Structure _____		
At-Grade Signals <input type="checkbox"/> Yes <input type="checkbox"/> No		
8. Bridges/Grade Separation Structures:		
<input checked="" type="checkbox"/> New Structure Length/Width <u>240/2</u>		\$85,000.00
Location <u>MSE Wall on West Side of Proposed Pathway</u>		
<input checked="" type="checkbox"/> Repair/Widening/Rehabilitation Length/Width <u>240/14</u>		\$438,000.00
Location <u>West Side of Existing Bridge</u>		
9. Traffic Items (Delineators, Signing, Channelization, Lighting, and Signals)		
10. Construction Traffic Control (Sign, Pavement Markings, Flagging, and Traffic Separation)		\$2,000
11. Detours		
12. Landscaping / Aesthetic Features		\$18,000
13. Mitigation Measures		\$9,000
14. Other Items (Roadside Development, Guardrail, Fencing, Sidewalks, Curb and Gutter, C.S.S. Items)		\$96,000
15. Cost of Constructions (Items 3 through 14)		\$707,000
16. Mobilization 10 % of Item 15		\$71,000
17. Construction Engineer and Contingencies 35 % of Items 15 and 16		\$272,000
18. Total Construction Cost (15 + 16 + 17)		\$1,050,000
19. Total Project Cost (1 + 2 + 18)		\$1,299,000
20. Project Cost Per Mile	\$1,000	\$10,825,000

Prepared By:

J-U-B Engineers, Inc.

ITEM NO.	ITEM DESCRIPTION	QTY	UNIT	UNIT PRICE	ROUNDED TOTAL
Pathway					
201-005A	Clearing & Grubbing	0.31	ACRE	\$ 50,000.00	\$ 16,000.00
203-015A	Rem of Bituminous Surf	56	SY	\$ 12.00	\$ 700.00
205-010A	Excavation Schedule No. 1	190	CY	\$ 25.00	\$ 5,000.00
205-045A	Granular Borrow	690	TON	\$ 25.00	\$ 17,000.00
205-060A	Water For Dust Abatement	25	MG	\$ 120.00	\$ 3,000.00
212-020A	Silt Fence	1400	FT	\$ 4.00	\$ 5,600.00
621-005A	Seed Bed Preparation	0.23	ACRE	\$ 3,400.00	\$ 800.00
621-010A	Seeding	0.23	ACRE	\$ 3,500.00	\$ 800.00
621-015A	Mulching	0.23	ACRE	\$ 2,800.00	\$ 600.00
621-025A	Mulch Anchoring (Tackifier)	0.23	ACRE	\$ 850.00	\$ 200.00
626-010A	Rent Const Sign CL B	112	SF	\$ 8.00	\$ 900.00
626-040A	Rent Const Barr CL B TY III	8	EACH	\$ 50.00	\$ 400.00
626-050A	Rent Drum CL B	45	EACH	\$ 18.00	\$ 800.00
S501-17A	MSE/Keystone Retaining Wall	1800	SF	\$ 47.00	\$ 85,000.00
S637-10A	Aggregate Base for Shared-Use Path	135	TON	\$ 46.00	\$ 6,200.00
S637-15A	Plantmix for Shared-Use Path	72	TON	\$ 190.00	\$ 14,000.00
S637-20A	Concrete for Shared-Use Path	400	SY	\$ 120.00	\$ 48,000.00
S901-05A	Aesthetic Features	1	LS	\$ 15,000.00	\$ 15,000.00
S904-05A	Contractor's Staging Area	1	LS	\$ 20,000.00	\$ 20,000.00
S911-05A	Ornamental Handrail 60"	373	FT	\$ 75.00	\$ 28,000.00
S501-25A	SP Bridge	1	LS	\$ 438,000.00	\$ 438,000.00
Z629-05A	Mobilization	1	LS	\$ 70,600.00	\$ 71,000.00
Contingency (~20%):					\$ 156,000.00
Construction Total:					\$ 933,000.00
Const. Engr. Total:					\$ 117,000.00
Right-Of-Way Total:					\$ 15,000.00
Engineering Total:					\$ 187,000.00
Grant Admin Total:					\$ 47,000.00
Project Total:					\$ 1,299,000.00

Environmental Screening

For Community Transportation Enhancement (CTE),
Safe Routes to School (SR2S) and Scenic Byway Projects



Background - All project actions which involve a federal nexus (federal funds, federal permits or federal lands) must have an approved environmental document. ITD follows Federal Highway Administration guidelines for environmental documentation.

Responsibility - ITD will be responsible for the review and approval of the environmental document. The sponsor is responsible for the preparation of the environmental document. Pre-application coordination with the district office (environmental) is needed. In some cases the sponsor may arrange for ITD to complete all or part of the environmental documentation.

Purpose of Form - This form is not an environmental clearance. The questions screen for issues that could require additional analysis or work. If you answer yes to any of the following questions, the environmental requirements or impacts may be greater than expected. The impacts may not be compatible with your budget or schedule. You should seek further assistance from ITD regarding the viability of the project.

Contacts - For assistance with the environmental process please contact the ITD District Environmental Planner. An abbreviated environmental clearance is available for pavement marking projects.

Answer the following questions and explain in detail any response that is not clear from simply marking the box. When completed electronically, the form will expand to allow room for explanations.

Project Type/Scope of Work (i.e., landscaping, bike/pedestrian path, etc.)	Project Name/Location
Bike/Pedestrian Bridge & Multi-Use Pathway Connections	Bike/Ped Bridge over North Channel of the Boise River

	<u>Yes</u>	<u>No</u>
<p>Right of Way/Property Impacts - Will the project require acquisition of temporary or permanent easements, or right of way? Is the project on, or through, federal lands or tribal lands? Will the project cause a temporary or permanent disruption to a commercial property or residential neighborhood?</p> <p style="margin-left: 20px;">An easement from Idaho Department of Lands will be needed given the bridge crosses over the Boise River. Additionally, consultation with the Shoshone Bannock Tribes will be required as part of the environmental process associated with federal funding. Commercial properties primarily lie north of the Boise River while residential communities are south of the project area. The addition of a new bicycle/pedestrian bridge is not anticipated to disrupt existing commercial operations or access for adjacent neighborhoods. The public would be notified of any potential bike/pedestrian detours.</p> <p style="margin-left: 20px;">*See Right of Way Encroachment section below for more detail.</p>	<input checked="" type="checkbox"/>	<input type="checkbox"/>
<p>Traffic - Does the project add traffic lanes or traffic capacity?</p> <p style="margin-left: 20px;">Explain: Currently, there are no plans to widen this section of Eagle Road/SH-55.</p>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
<p>Ground Disturbance - Does the project disturb more than one acre of land?</p> <p style="margin-left: 20px;">Explain: Total ground disturbance is unknown at this time, but will be determined during preliminary design. An NPDES Storm Water Discharge Permit will be required if ground disturbance is greater than one acre and if storm water is to be discharged into Waters of the U.S.</p>	<input type="checkbox"/>	<input type="checkbox"/>
<p>Stormwater - Where does the water (rain, snowmelt) from this project area drain?</p> <ul style="list-style-type: none"> <input checked="" type="checkbox"/> Sheet flows to surface waters (canal, stream, lake) <input type="checkbox"/> Conveyed by ditch or pipe to surface waters <input type="checkbox"/> Storm Sewer System (Municipal system) <input type="checkbox"/> Infiltrate in Place (retention pond or topography with no drainage outlet [low area]) <input type="checkbox"/> Other – if none of the above conditions <p style="margin-left: 20px;">Explain:</p>		
<p>Surface Waters - Does the project site contain any boggy, swampy, or wetland areas?</p> <p style="margin-left: 20px;">Does the project impact (fill or temporarily impact) any wetland, stream, lake or other water body?</p> <p style="margin-left: 20px;">Explain: The proposed, new 12-foot wide bike/ped bridge will join and widen the existing State Highway 55/Eagle Road bridge, crossing the Boise River. This will be accomplished by attaching precast concrete beams supported by extending the existing piers and abutments with additional pile foundations. The necessary instream work required</p>	<input checked="" type="checkbox"/>	<input type="checkbox"/>

for the new piles and piers will necessitate a greater level of coordination between the State and the City of Eagle for construction and maintenance of the pedestrian structure.

The length of the bike-pedestrian bridge will be the same as the existing 240 ft. long bridge to maintain the existing channel opening and to accommodate the existing east-west path at the north side of the river. The bike/pedestrian bridge will also have the same span lengths as the existing bridge in order to: maintain the existing stream hydraulic behavior; maintain a "no-rise" impact on the river for the additional in-stream bike/pedestrian piers; and to minimize any changes that would increase the potential for scouring at the existing abutments and piers. The proposed abutments and piers for the proposed bike/pedestrian bridge would be constructed abutted against the existing bridge abutments and piers while maintaining structural independence from each other with small joint between the existing and proposed structures.

Stream Alteration/Encroachment [COE (404)]:

Section 404 of the Clean Water Act requires that a Department of Army (DA) permit be obtained from the U.S. Army Corps of Engineers (USACE) for the discharge of dredged and/or fill material into waters of the U.S., including wetlands. Waters of the U.S. include most perennial and intermittent rivers and streams, natural and man-made lakes and ponds, as well as irrigation and drainage canals and ditches that are tributaries to other waters, and wetlands.

A Water Resources Assessment will need to be prepared by a qualified biologist to evaluate potential affects. Future consultation with the USACE will be required given the scope of work within the Boise River, which are considered to be jurisdictional waterways. In summary, a Preliminary Jurisdictional Determination (PJD) from the U.S. Corps of Engineers will be necessary and a section 404 permit will be required for the project.

Cultural Resources - Are there historical structures (such as buildings, bridges, canals, etc) over 45 years old within or adjacent to (in some cases within view) of the proposed project site?

Historic Structures:

The Boise River North Channel State bridge was constructed in 1978 and one other adjacent State bicycle/pedestrian bridge in 1998 within the immediate project area. Neither bridge is over 45 years old and eligible under the National Register of Historic Places. A total of five National Register of Historic Places are listed in Eagle, Idaho. All five properties are located north of State Street/Highway 44, outside the vicinity of proposed project area. Consultation with the Shoshone Bannock Tribes as part of the environmental process will be required for any work associated with the Boise River. A qualified Cultural Resource Specialist will need to prepare a Cultural Resources Report to evaluate potential impacts to any historic or 4(f) resources.

Explain:

*See attached National Register of Historic Places map.

Wetlands:

Freshwater Emergent and Freshwater Forrested/Shrub Wetlands are present within the project area as shown in the attached USFWS National Wetlands Inventory map. Given the proximity of the present wetlands to the proposed bridge project area, special care and consideration including a Water Resources Assessment and Wetland Delineation will need to be prepared by a qualified biologist.

Section 4f - Is the project site located next to or a part of a special designated land use (i.e., designated park, wildlife refuge, historic district, etc)? Check with local land use map for information.

Section 4(f) protects the following basic types of properties: publicly owned park and recreation areas that are open to the general public, publicly owned wildlife and waterfowl refuges, and public or privately owned historic sites. The term historic sites includes prehistoric and historic districts, sites, buildings, structures or objects listed in, or eligible for, the National Register of Historic Places. This may also include places of traditional religious and cultural importance to an Indian tribe or Native Hawaiian organization and that meet the National Register criteria.

Explain: FHWA decides whether Section 4(f) applies to a resource, reviews assessments of each alternative's impacts to 4(f) properties, and determines whether the law allows the selection of a particular alternative after consulting with the Department of Interior.

In the case of this project, the Boise River Greenbelt is likely considered a Section 4(f) property due to its recreational purpose. The Greenbelt is an east-west multi-use pathway system that traverses several miles along the Boise River through the Cities of Boise, Garden City and parts of Eagle. Impacts to a public park, recreation area, or wildlife and waterfowl refuge may be determined to be de minimis if the transportation use of the Section 4(f) property, including incorporation of any measure(s) to minimize harm (such as any avoidance, minimization,

mitigation, or enhancement measures), does not adversely affect the activities, features, or attributes that qualify the resource for protection under Section 4(f). The proposed bridge will provide improved and expanded opportunities for the public to use the existing Greenbelt pathway network.

The impacts of a transportation project on a park, recreation area, or wildlife and waterfowl refuge that qualifies for Section 4(f) protection may be determined to be de minimis if:

1. The transportation use of the Section 4(f) property, together with any impact avoidance, minimization, and mitigation or enhancement measures incorporated into the project, does not adversely affect the activities, features, or attributes that qualify the resource for protection under Section 4(f);
2. The public has been afforded an opportunity to review and comment on the effects of the project on the protected activities, features, or attributes of the Section 4(f) property; and
3. The official(s) with jurisdiction over the property, after being informed of the public comments and FHWA's intent to make the de minimis impact finding, concur in writing that the project will not adversely affect the activities, features, or attributes that qualify the property for protection under Section 4(f).

Hazardous Waste - Is there any indication of waste spill or stain on the project site? Are there any gas stations, dry cleaner, or other industrial facilities adjacent to the project?

A total of 8 wastesites, including Underground Storage Tanks (UST) and Leaking Underground Storage Tanks (LUST) have been identified within Eagle City limits. However, none of these sites are located directly within the

Explain: project limits. The nearest gas station (Chevron) is approximately .23 miles from the project site.

*See attached hazardous waste sites map.

Public Involvement – Based on your public involvement, has any public controversy or issue been identified? Do you anticipate any temporary or permanent disruption to a commercial property or residential neighborhood (access changes or detours, construction noise etc?)

Public Involvement activities are designed to inform, educate and solicit input from project area stakeholders regarding the Eagle Road Bike/Ped Bridge project. In addition to public education efforts, proper support for stakeholder communications to ensure questions and concerns are addressed in a timely manner will be vital to the success of the project.

Summary of key public involvement actions:

- Explain:
- a) Situation Assessment/Stakeholder Interviews
 - b) Develop a Bridge Committee of Key Stakeholders
 - c) Utilize News Releases with Local Media Outlets
 - d) Public Open Houses (at least two)
 - e) Stakeholder Coordination
 - f) Public Official Coordination

*See the attached Public Involvement Plan for further details.

Irrigation - Does the project require irrigation? Describe whether the project will require watering and what source will be used for watering.

Explain: The bicycle/pedestrian bridge will not require irrigation watering.

Right of Way Encroachment - Are there any signs, trees or other features you plan to locate within ITD right of way?

The current proposal for a 14-foot westerly extension of the existing bridge will leave approximately 23 feet between the west edge of the new bridge and the westerly ITD right-of-way line over the river. If more than 23 feet is needed to construct the new footings, a temporary easement from Idaho Department of Lands (IDL) & the Mace River Ranch Homeowners Association would be necessary.

Explain: The southerly tie-in, as currently proposed, would also be within the existing right-of-way (ROW) and, additionally, the adjacent property owner is the City of Eagle.

The northerly tie-in, as currently proposed, would be within the existing right-of-way but the end of the return falls right at the northerly edge of the IDL 25-foot greenbelt easement. The existing pathways running west and north from the intersection currently fall partially outside the IDL greenbelt easement and outside the existing platted pedestrian easement (reserved to the Channel Center Subdivision Property Owner's Association). Access to

Yes **No**

construct the northerly tie-in may necessitate obtaining a temporary access & construction easement from the owner of Lot 1, Block 1 of Channel Center Subdivision (BB One LLC).

*See attached ROW Map

Offsite Work - Will the project require off-site grading, excavation or trenching for utilities, lighting, drainage or other work?

Explain: All off-site work including grading, excavation, trenching for utilities, lighting or drainage will be determined during preliminary design.

Describe any other known or suspected environmental issue that has not been covered U.S. Fish and Wildlife Service (USFWS):

An official species list for threatened and endangered species was requested on February 1, 2016 from USFWS . A total of 1 threatened or endangered species (Slickspot peppergrass - proposed endangered) is identified within the project area and should be considered in an effects analysis for this project. There are no critical habitats listed within the project area.

Preparer's Printed Name	Title	Agency or Firm
Travis Jeffers	Planner	J-U-B Engineers, Inc.
Signature		Date
		2/5/2016

- ITD Use Only -

Recommendation

- Based on the information in the project application and on this form, the project is likely to be eligible for a Categorical Exclusion.
- Based on the information in the project application and on this form, there were environmental areas of concern that should be further discussed prior to funding this project.
- There was not enough information in the project application and on this form to assess potential environmental issues.

Comment

Printed Name	Title	
	District Environmental Planner	
Signature		Date

Environmental Resource Information



LWCF Home

Report Home

NPS ParkNet

Main Report

- Ada
- BANNOCK
- BEAR LAKE
- BENEWAH
- BINGHAM
- BLAINE
- BOISE
- Bonner
- BONNEVILLE
- BOUNDARY
- BUTTE
- CAMAS
- CANYON
- CARIBOU
- CASSIA
- CLEARWATER
- Custer
- ELMORE
- FRANKLIN
- FREMONT
- GEM
- GOODING
- IDAHO
- JEFFERSON
- JEROME
- Kootenai
- LATAH
- LEMHI
- LEWIS
- LINCOLN
- MADISON
- MINDOKA
- MULTI-COUNTY
- NEZ PERCE
- ONEIDA
- OWYHEE
- PAYETTE
- PLANNING
- Power
- SHOSHONE
- TWIN FALLS
- VALLEY
- WASHINGTON

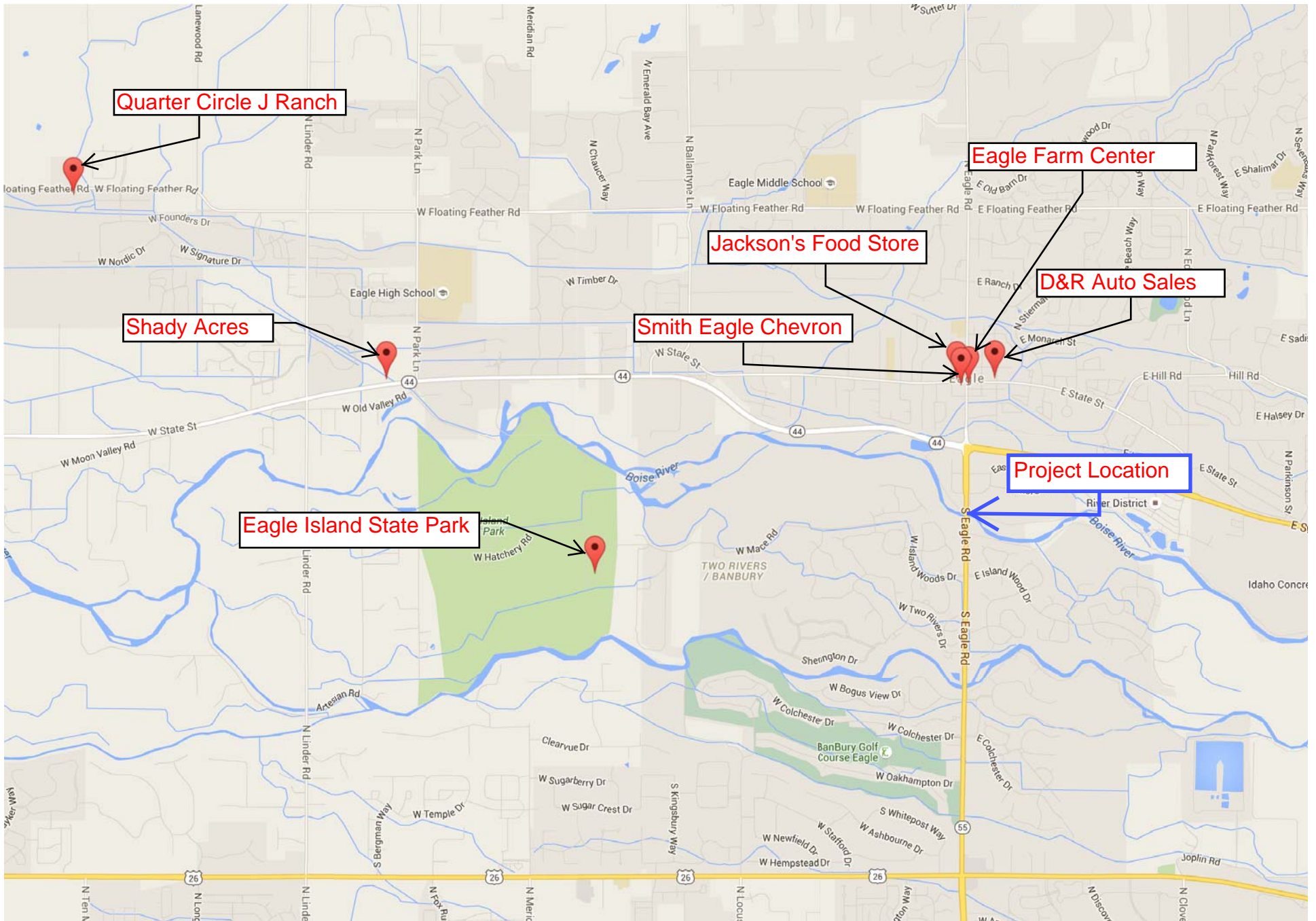
United States Department of the Interior
National Park Service
Land & Water Conservation Fund

Detailed Listing of Grants Grouped by County

IDAHO - 16

Today's Date: 2/1/2016 Page: 1

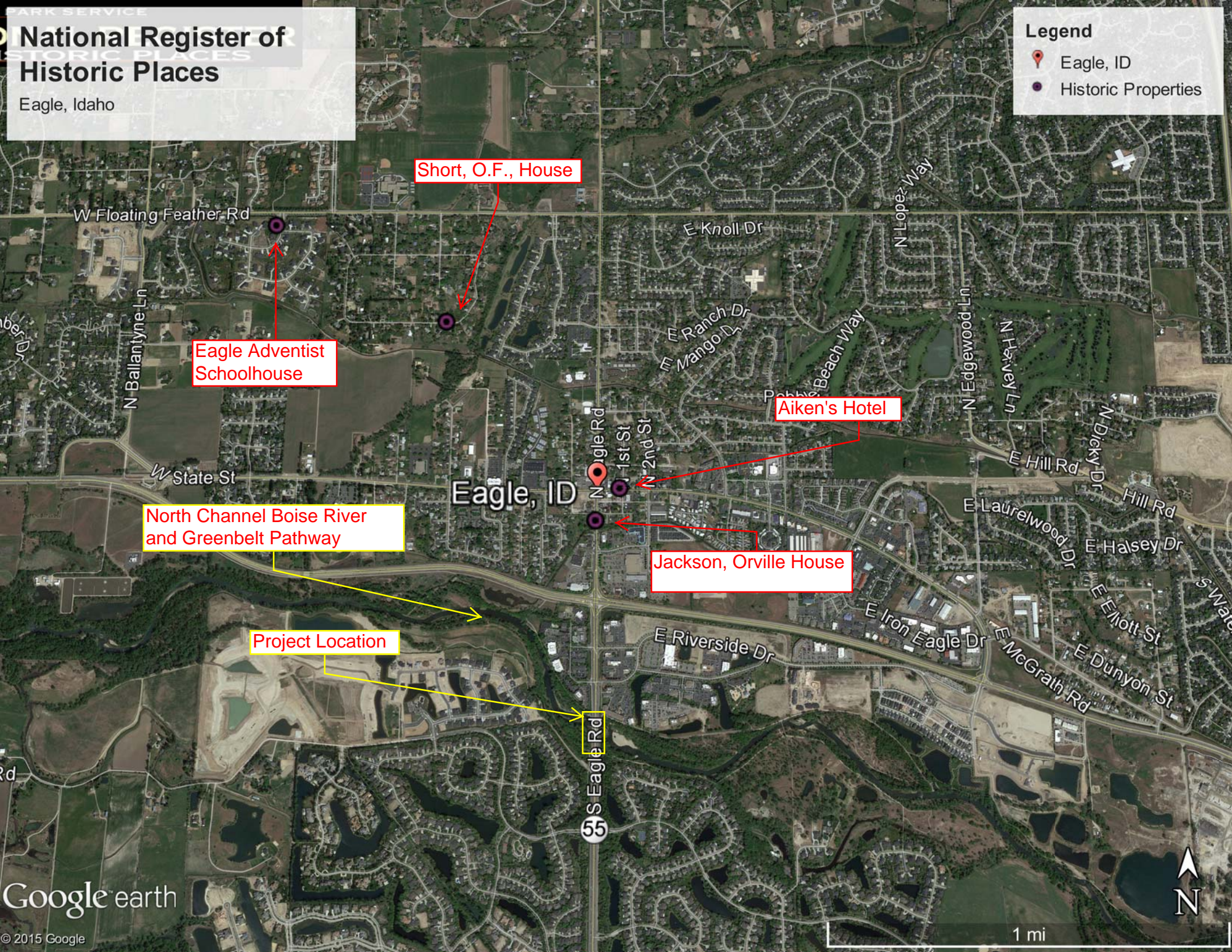
Grant ID & Element	Type	Grant Element Title	Grant Sponsor	Amount	Status	Date Approved	Exp. Date	Cong. District
ADA								
10 - XXX	A	ACQUIRE BOISE GAME RANGE AND RECREATION AREA	ID DEPARTMENT OF FISH AND GAME	\$22,500.00	C	2/23/1966	6/30/1968	1
24 - XXX	D	CITY OF BOISE GREENBELT IMPROVEMENT	CITY OF BOISE	\$7,838.50	C	8/20/1966	1/1/1969	2
25 - XXX	D	LADY BIRD PARK	ADA COUNTY	\$49,493.39	C	8/20/1966	9/1/1968	1
34 - XXX	A	DON STOREY PARK (ACQUISITION)	CITY OF MERIDIAN	\$13,905.00	C	12/6/1966	11/15/1968	1
36 - XXX	A	BOISE GAME RANGE ACQUISITION	ID DEPARTMENT OF FISH AND GAME	\$30,900.00	C	1/3/1967	6/30/1968	1
53 - XXX	A	BOISE RIVER GREENBELT	CITY OF BOISE	\$121,803.88	C	8/20/1968	1/1/1971	2
97 - XXX	A	BOISE GREENBELT	CITY OF BOISE	\$209,563.35	C	6/15/1971	7/1/1977	2
109 - XXX	D	BOISE GREENBELT DEVELOPMENT	CITY OF BOISE	\$94,934.19	C	7/20/1971	7/1/1975	2
143 - XXX	D	SOUTH BOISE PARK	CITY OF BOISE	\$39,635.63	C	7/6/1972	6/1/1975	1
146 - XXX	D	FLYING HAWK PARK	CITY OF BOISE	\$16,484.77	C	6/21/1972	3/1/1977	1
147 - XXX	D	CASSIA PARK	CITY OF BOISE	\$23,131.99	C	7/20/1972	6/1/1976	2
148 - XXX	A	BARBER PARK	ADA COUNTY	\$7,742.74	C	6/14/1972	4/30/1973	1
166 - XXX	D	WESTERN ADA RECREATION DISTRICT POOL	WESTERN ADA RECREATION DISTRICT	\$113,690.43	C	10/25/1972	12/1/1974	1
171 - XXX	D	FAIRMONT PARK DEVELOPMENT	CITY OF BOISE	\$150,030.04	C	12/29/1972	12/1/1976	1
172 - XXX	D	HILLSIDE PARK DEVELOPMENT	CITY OF BOISE	\$84,851.06	C	2/6/1973	12/1/1976	1
196 - XXX	C	BARBER PARK PHASE III	ADA COUNTY	\$50,017.15	C	7/5/1973	6/1/1975	1
197 - XXX	D	SOUTH BOISE PARK (PHASE II)	CITY OF BOISE	\$35,750.15	C	9/25/1973	9/30/1975	1
202 - XXX	D	CASSIA PARK (PHASE II)	CITY OF BOISE	\$80,474.63	C	10/10/1973	10/1/1976	2
205 - XXX	D	ADA COUNTY HIGHWAY DISTRICT BIKEWAY	ADA COUNTY	\$16,859.86	C	3/20/1974	5/1/1976	1
206 - XXX	D	BARBER PARK DEVELOPMENT	ADA COUNTY	\$51,530.30	C	4/2/1974	5/1/1976	1
207 - XXX	D	STATE HIGHWAY BICYCLE SYSTEM	ADA COUNTY	\$112,846.37	C	4/2/1974	6/30/1979	1



National Register of Historic Places
Eagle, Idaho

Legend

-  Eagle, ID
-  Historic Properties



Short, O.F., House

Eagle Adventist Schoolhouse

Aiken's Hotel

North Channel Boise River and Greenbelt Pathway

Jackson, Orville House

Project Location

Eagle, ID

55 S Eagle Rd





United States Department of the Interior



FISH AND WILDLIFE SERVICE
Idaho Fish and Wildlife Office
1387 SOUTH VINNELL WAY, SUITE 368
BOISE, ID 83709
PHONE: (208)378-5243 FAX: (208)378-5262

Consultation Code: 01EIFW00-2016-SLI-0324

February 01, 2016

Event Code: 01EIFW00-2016-E-00337

Project Name: Bike/Ped Bridge over North Channel of the Boise River

Subject: List of threatened and endangered species that may occur in your proposed project location, and/or may be affected by your proposed project

To Whom It May Concern:

The enclosed species list identifies threatened, endangered, proposed and candidate species, as well as proposed and final designated critical habitat, that may occur within the boundary of your proposed project and/or may be affected by your proposed project. The species list fulfills the requirements of the U.S. Fish and Wildlife Service (Service) under section 7(c) of the Endangered Species Act (Act) of 1973, as amended (16 U.S.C. 1531 *et seq.*).

New information based on updated surveys, changes in the abundance and distribution of species, changed habitat conditions, or other factors could change this list. Please feel free to contact us if you need more current information or assistance regarding the potential impacts to federally proposed, listed, and candidate species and federally designated and proposed critical habitat. Please note that under 50 CFR 402.12(e) of the regulations implementing section 7 of the Act, the accuracy of this species list should be verified after 90 days. This verification can be completed formally or informally as desired. The Service recommends that verification be completed by visiting the ECOS-IPaC website at regular intervals during project planning and implementation for updates to species lists and information. An updated list may be requested through the ECOS-IPaC system by completing the same process used to receive the enclosed list.

The purpose of the Act is to provide a means whereby threatened and endangered species and the ecosystems upon which they depend may be conserved. Under sections 7(a)(1) and 7(a)(2) of the Act and its implementing regulations (50 CFR 402 *et seq.*), Federal agencies are required to utilize their authorities to carry out programs for the conservation of threatened and endangered species and to determine whether projects may affect threatened and endangered species and/or designated critical habitat.

A Biological Assessment is required for construction projects (or other undertakings having

similar physical impacts) that are major Federal actions significantly affecting the quality of the human environment as defined in the National Environmental Policy Act (42 U.S.C. 4332(2)(c)). For projects other than major construction activities, the Service suggests that a biological evaluation similar to a Biological Assessment be prepared to determine whether the project may affect listed or proposed species and/or designated or proposed critical habitat. Recommended contents of a Biological Assessment are described at 50 CFR 402.12.

If a Federal agency determines, based on the Biological Assessment or biological evaluation, that listed species and/or designated critical habitat may be affected by the proposed project, the agency is required to consult with the Service pursuant to 50 CFR 402. In addition, the Service recommends that candidate species, proposed species and proposed critical habitat be addressed within the consultation. More information on the regulations and procedures for section 7 consultation, including the role of permit or license applicants, can be found in the "Endangered Species Consultation Handbook" at:

<http://www.fws.gov/endangered/esa-library/pdf/TOC-GLOS.PDF>

Please be aware that bald and golden eagles are protected under the Bald and Golden Eagle Protection Act (16 U.S.C. 668 *et seq.*), and projects affecting these species may require development of an eagle conservation plan (http://www.fws.gov/windenergy/eagle_guidance.html). Additionally, wind energy projects should follow the wind energy guidelines (<http://www.fws.gov/windenergy/>) for minimizing impacts to migratory birds and bats.

Guidance for minimizing impacts to migratory birds for projects including communications towers (e.g., cellular, digital television, radio, and emergency broadcast) can be found at: <http://www.fws.gov/migratorybirds/CurrentBirdIssues/Hazards/towers/towers.htm>; <http://www.towerkill.com>; and <http://www.fws.gov/migratorybirds/CurrentBirdIssues/Hazards/towers/comtow.html>.

Please note: The IPaC module for producing a list of proposed and designated critical habitat is currently incomplete. At this time, we ask that you use the information given below to determine whether your action area falls within a county containing proposed/designated critical habitat for a specific species. If you find that your action falls within a listed county, use the associated links for that species to determine if your action area actually overlaps with the proposed or designated critical habitat.

Canada Lynx (*Lynx canadensis*) - Designated February 24, 2009.
Counties: Boundary County.

Federal Register Notice:

<http://www.gpo.gov/fdsys/pkg/FR-2009-02-25/pdf/E9-3512.pdf#page=1>

Printable Maps:

http://www.fws.gov/mountain-prairie/species/mammals/lynx/criticalhabitat_files/20081222_fedre

GIS Data: http://criticalhabitat.fws.gov/docs/crithab/zip/lynx_ch.zip

KML for Google Earth: (None Currently Available)

Selkirk Mountains Woodland Caribou (*Rangifer tarandus Caribou*) - Proposed November 30, 2011.

Counties: Bonner and Boundary Counties.

Federal Register Notice: <http://www.fws.gov/idaho/home/2011-30451FINALR.pdf>

Printable Maps: http://www.fws.gov/idaho/home/Map1_sub1_150.pdf

GIS Data: (None Currently Available)

KML for Google Earth: (None Currently Available)

Bull Trout (*Salvelinus confluentus*) - Designated September 30, 2010.

Counties: Adams, Benewah, Blaine, Boise, Bonner, Boundary, Butte, Camas, Clearwater, Custer, Elmore, Gem, Idaho, Kootenai, Lemhi, Lewis, Nez Perce, Owyhee, Shoshone, Valley, and Washington Counties.

Federal Register Notice:

<http://www.gpo.gov/fdsys/pkg/FR-2010-10-18/pdf/2010-25028.pdf#page=2>

Printable Maps: http://www.fws.gov/pacific/bulltrout/CH2010_Maps.cfm#CHMaps

GIS Data: <http://criticalhabitat.fws.gov/docs/crithab/zip/bulltrout.zip>

KML for Google Earth:

http://www.fws.gov/pacific/bulltrout/finalcrithab/BT_FCH_2010_KML.zip

Kootenai River White Sturgeon (*Acipenser transmontanus*) - Designated July 9, 2008.

Counties: Boundary County.

Federal Register Notice:

<http://www.gpo.gov/fdsys/pkg/FR-2008-07-09/pdf/E8-15134.pdf#page=1>

Printable Maps: (None Currently Available)

GIS Data: http://criticalhabitat.fws.gov/docs/crithab/zip/fch_73fr39506_acit_2009.zip

KML for Google Earth: (None Currently Available)

Slickspot Peppergrass (*Lepidium papilliferum*) - Proposed May 10, 2011. Counties: Ada, Canyon, Elmore, Gem, Owyhee, and Payette Counties.

Federal Register Notice: <http://www.gpo.gov/fdsys/pkg/FR-2011-10-26/pdf/2011-27727.pdf>

Printable Maps: <http://www.fws.gov/idaho/Lepidium.html>

GIS Data: (None Currently Available)

KML for Google Earth: (None Currently Available)

We appreciate your concern for threatened and endangered species. The Service encourages Federal agencies to include conservation of threatened and endangered species into their project planning to further the purposes of the Act. Please include the Consultation Tracking Number in the header of this letter with any request for consultation or correspondence about your project that you submit to our office.

Attachment



United States Department of Interior
Fish and Wildlife Service

Project name: Bike/Ped Bridge over North Channel of the Boise River

Official Species List

Provided by:

Idaho Fish and Wildlife Office
1387 SOUTH VINNELL WAY, SUITE 368
BOISE, ID 83709
(208) 378-5243

Consultation Code: 01EIFW00-2016-SLI-0324

Event Code: 01EIFW00-2016-E-00337

Project Type: BRIDGE CONSTRUCTION / MAINTENANCE

Project Name: Bike/Ped Bridge over North Channel of the Boise River

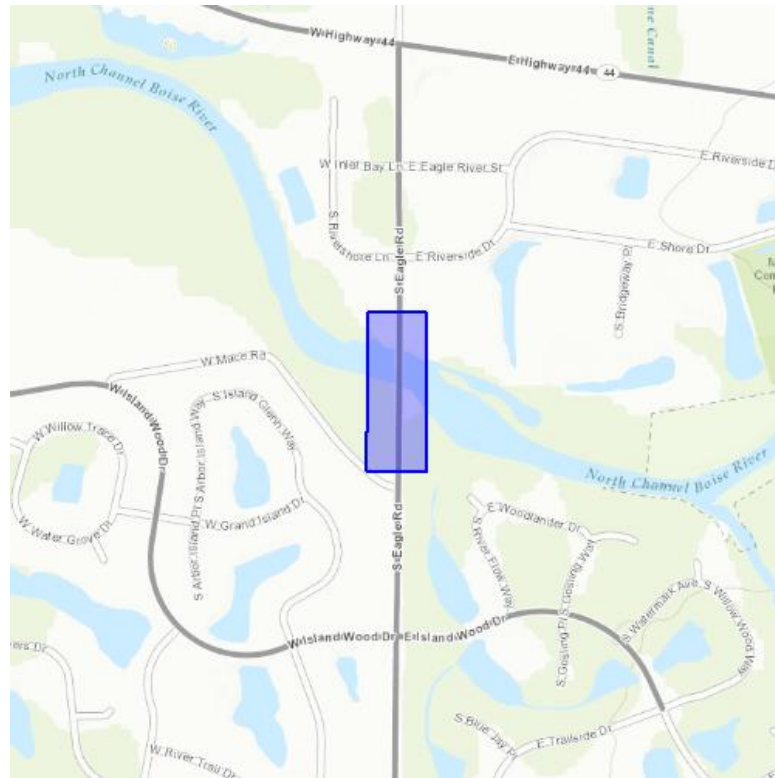
Please Note: The FWS office may have modified the Project Name and/or Project Description, so it may be different from what was submitted in your previous request. If the Consultation Code matches, the FWS considers this to be the same project. Contact the office in the 'Provided by' section of your previous Official Species list if you have any questions or concerns.



United States Department of Interior
Fish and Wildlife Service

Project name: Bike/Ped Bridge over North Channel of the Boise River

Project Location Map:



Project Coordinates: MULTIPOLYGON (((-116.3547120095609 43.687149323692026, -116.35354041987738 43.687149323692026, -116.35353613070036 43.684879214789305, -116.35472273839694 43.68489628322219, -116.3547120095609 43.687149323692026)))

Project Counties: Ada, ID



United States Department of Interior
Fish and Wildlife Service

Project name: Bike/Ped Bridge over North Channel of the Boise River

Endangered Species Act Species List

There are a total of 1 threatened or endangered species on your species list. Species on this list should be considered in an effects analysis for your project and could include species that exist in another geographic area. For example, certain fish may appear on the species list because a project could affect downstream species. Critical habitats listed under the **Has Critical Habitat** column may or may not lie within your project area. See the **Critical habitats within your project area** section further below for critical habitat that lies within your project. Please contact the designated FWS office if you have questions.

Flowering Plants	Status	Has Critical Habitat	Condition(s)
Slickspot peppergrass (<i>Lepidium papilliferum</i>)	Proposed Endangered	Proposed	



United States Department of Interior
Fish and Wildlife Service

Project name: Bike/Ped Bridge over North Channel of the Boise River

Critical habitats that lie within your project area

There are no critical habitats within your project area.